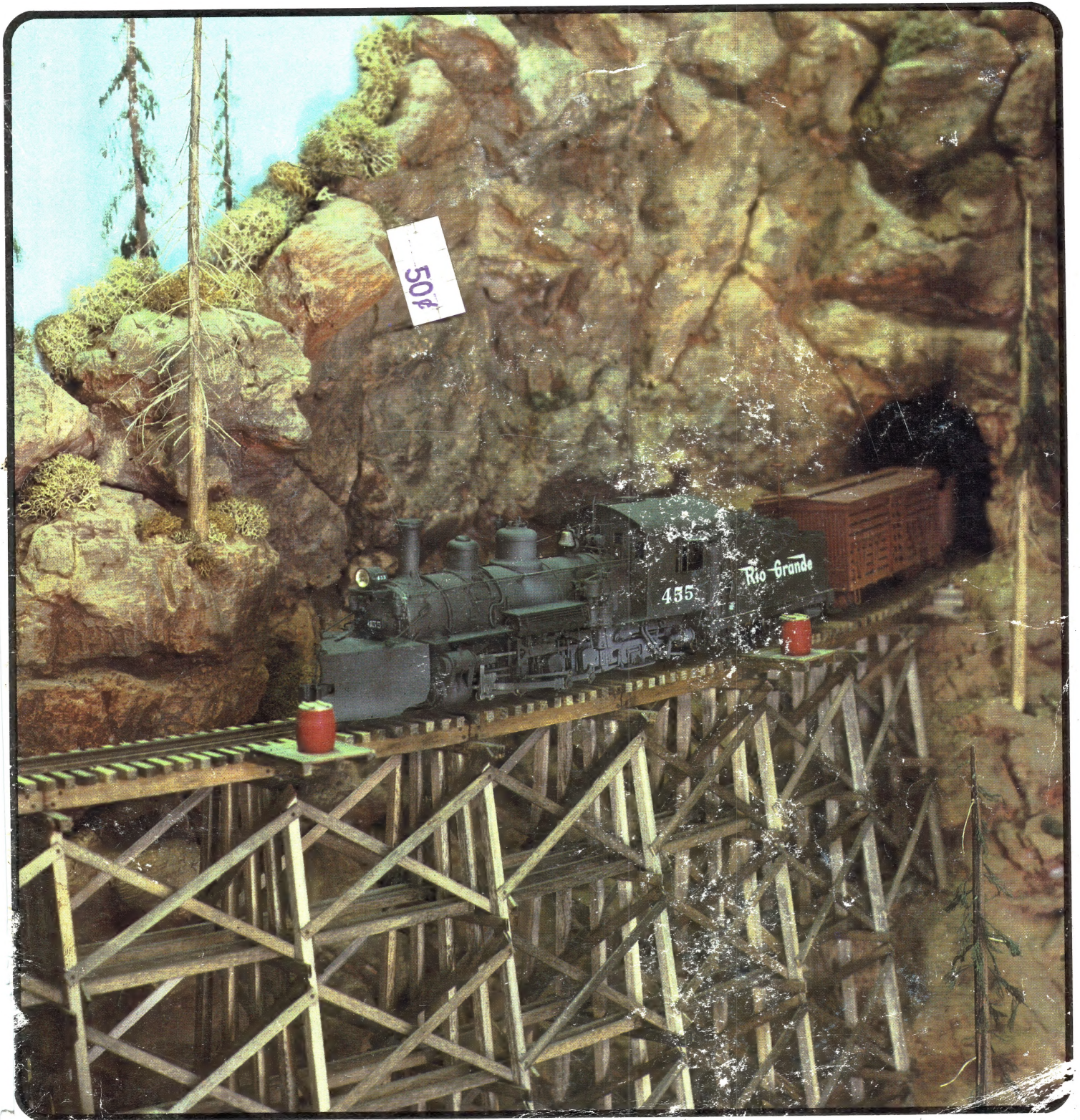


NARROW GAUGE AND SHORT LINE GAZETTE

Accurate Information for Fine Modelbuilding—Sep/Oct 1978 \$2.25



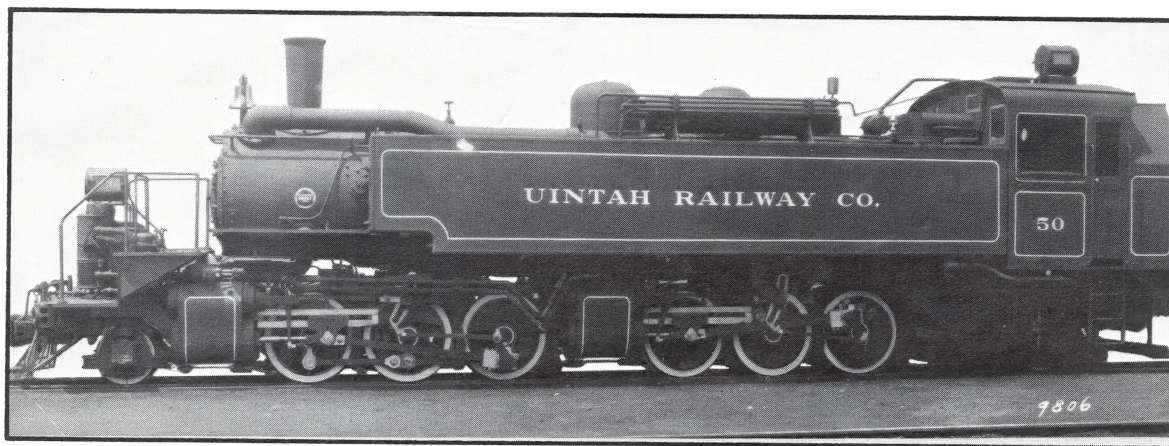
Uintah Railway

Nos. 50 and 51

2-6-6-2T

On3 from... **GOM**
scale model

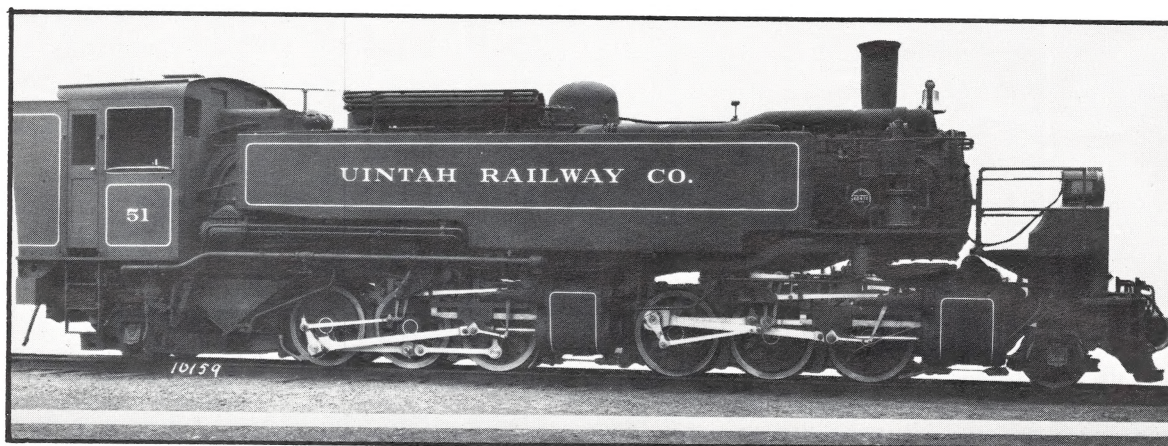
The first On3 model in our **CB Royale****** series.



These models will be meticulously detailed with castings by the Back Shop and featuring full cab interior detail.

There will be two distinctly different models, each following the prototype faithfully No. 50 in the early years, No. 51 in the later years.

A superb model by Japan's master craftsman Goto-san.



Available at your favorite narrow gauge dealer in late fall.

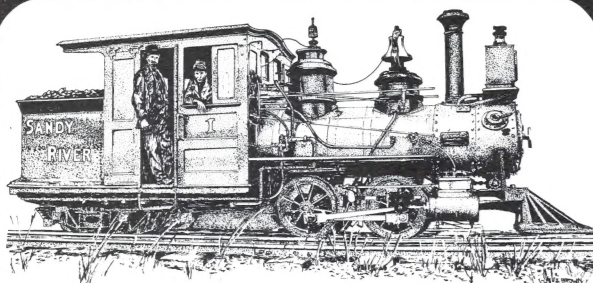
NARROW GAUGE MODELS OF SUPERIOR QUALITY

GOM
scale model



CB Royale****

22 West Nicholai Street, Hicksville, N.Y. 11801



Robert W. Brown
Editor & Publisher

Irene L. Brown, Business & Office Manager

Gene E. Deimling, Associate Editor
Charles W. Getz, Associate Editor

Contributors

Richard Andrews	Irv Schulz
Alan Armitage	Paul Scoles
Charles Givens	Lonnie Shay
Bill Jensen	Robert Sloan
Gordon North	Ted Wurm

The **NARROW GAUGE** and **SHORTLINE GAZETTE** is published bimonthly in March, May, July, September, November and January by **Gazette Publications**, One First Street, Suite C, P.O. Box 26, Los Altos, CA 94022.

SUBSCRIPTION RATES

United States Rates
2nd class mail \$12.50 per year
1st class mail \$20.00 per year

Foreign Rates (including Canada)
Surface \$15.00 per year
Air, Canada \$22.00 per year
Air, Europe \$25.00 per year
Air, Pacific \$29.00 per year

(U.S. funds please)

DEALER INFORMATION

Dealers write for dealer terms directly to **Gazette Publications**, P.O. Box 26, Los Altos, CA 94022 U.S.A.

GAZETTE PUBLICATIONS
One First Street, Suite C
P.O. Box 26
Los Altos, CA 94022

All correspondence should be sent to the above address.

Second Class postage paid at Los Altos, CA 94022 and at additional entry offices.

Printed in U.S.A. Copyright 1978, Gazette Publications. All rights reserved.

SPECIAL FEATURE

- 26 **Northern Narrow Gauge**
The White Pass & Yukon.
by *David G. Steer*

FEATURES

- 33 **Pickles and Vinegar on the Hi-Iron**
The Heinz Pickle Cars.
by *Irv Schulz*
- 36 **Rio Grande Southern #455 in Sn3**
A lost art revived.
by *Paul Washburn*
- 39 **Colorado Narrow Gauge Tank Cars, Part 2**
The TEXACO cars.
by *Robert Sloan*
- 42 **Finescale ON3 Rebuilding Techniques**
We dare you!
by *Kelly Morris*
- 47 **Pandora Abridged**
Love at first sight
by *F.L. Oster*
- 50 **The Modelmakers Notebook**
Something different in roofs.
by *Al Armitage*
- 54 **The Silverton Northern Railbus in Brass**
"Casey" rides again.
by *Bruce Eaton*

PLANS

- 30 **White Pass & Yukon Wooden Caboose**
by *David Steer*
- 31 **White Pass & Yukon Wooden Box Car**
by *David Steer*
- 32 **White Pass & Yukon Wooden Refrigerator Car**
by *David Steer*
- 39 **TEXACO Single Dome Tank Car**
by *John Maxwell*
- 40-41 **TEXACO Two Dome Tank Car #71**
by *Robert Sloan*
- 62-63 **Monson Junction Freight Station**
by *Al Tenney*

DEPARTMENTS

- 5 **Robert's Ramblings**
- 7 **The Pigeon Hole**
- 16 **New In Review**
- 22 **The Narrow Gauge Scene**
- 56 **Straight Talk About Bent Pipes**
- 58 **Gazette Gallery**
- 62 **Notes for the Extremely Narrow Minded**
- 69 **Book Reviews**
- 76 **Riptrack**

Cover

The talents of three modelers went into the creation of this cover. Paul Washburn built the beautiful Sn3 RGS K-27 #455 (see Blue Ribbon Series in this issue). John Ullmon built the layout which is used to frame the locomotive so effectively and Dick Christ took the color transparency.

Autopulse

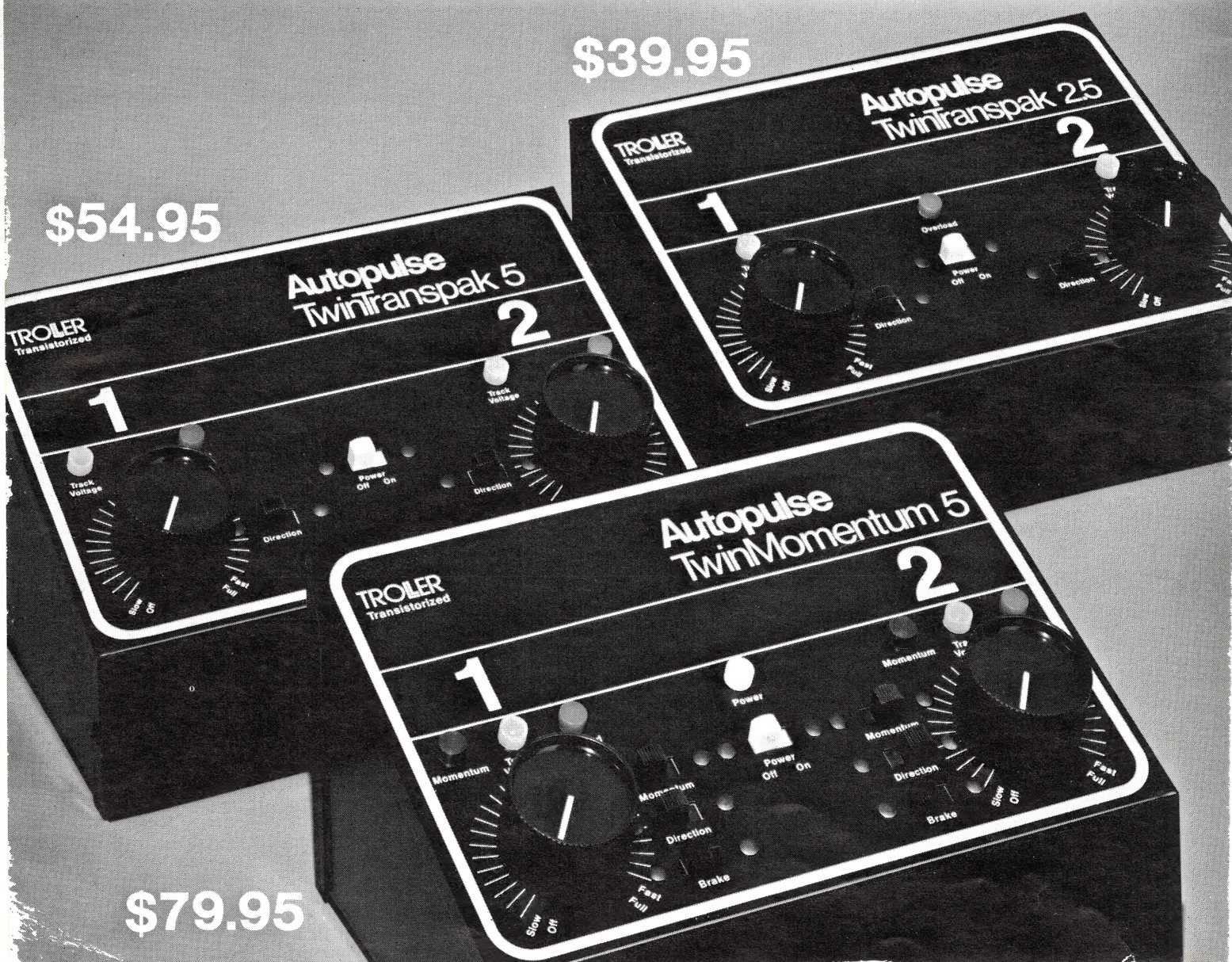
Twin, transistorized, timely... another Autopulse first

For years model railroaders have been asking for twin transistorized power packs, but they've simply not been available. Ask no more . . . **Autopulse** continues its transistorized power revolution by introducing 3 twin track models at less cost than you'd expect for a one track pack.

Depending on the model, **Autopulse Twin** packs feature Z, N, HO, S, ON3, O and LGB scale operation, up to 5 amps of transistorized power, common rail operation, momentum, variable braking, lighted monitors, UL listing to the stricter 1977 standards, and a Full 5 Year Warranty.

You will be amazed what technology has done to value. See your favorite hobby dealer or send SSAE for free brochure of the entire line.

Autopulse . . . a product of
Troller Corporation
4445 N. Ravenswood Avenue,
Chicago, Illinois 60640





Robert's Ramblings....

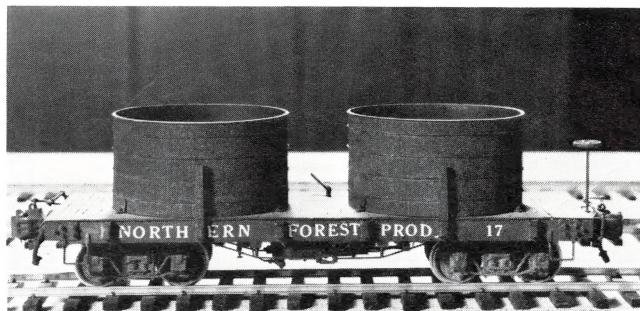
Those of you calling the **GAZETTE** office may have noticed a new voice at the end of the line. My wife, Irene, is currently managing the office as well as handling the business end of the magazine. Suzanne Olson, our previous office manager has left us to get married and return to school. As old married folk and educators, Irene and I applaud Suzanne's decision but we will miss her help. Best wishes and good luck Suzanne!

Those of you who were not on vacation probably noticed that your July issue was mailed late. This is partially my fault (for going on vacation to Colorado narrow gauge country) and partly the fault of the data service for printing the subscriber mailing labels late. A sincere apology to those who got their issue late. After all, everyone needs a vacation!

This issue introduces two new "occasional" series. The first is by that well known rebuilder of On3 locomotives Kelly Morris. Kelly has done some beautiful rebuilds and has some interesting techniques to share with all of us. His series is intended to show you some of the methods he uses for rebuilding locos. It is not intended to be a step by step series on how to rebuild a certain locomotive. However, Kelly does use a C-19 rebuild as a detailed example of his craft.

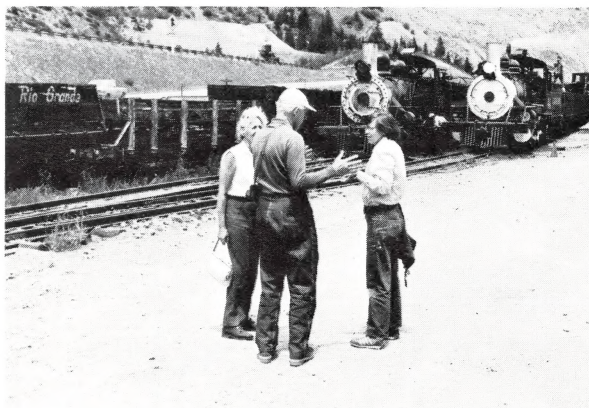
Bruce Eaton is the other occasional series author. Bruce works in HOn3 and has several articles on file with us. I suggest you read his article on the Casey Jones with an eye for picking up techniques unless of course you have a set of Sloan etchings you want to assemble.

The May/June 1978 issue of the **GAZETTE** had only been out a few days when the photograph below appeared in the mail. Remember how Gene Diemling asked about model maple sugar cars in regard to that photo in the Gallery? Well, Daryl Townsend (Town Models) sent in this photo of an On3 car modeled after a prototype used on the Northern Forest Products Line in Canada. It was a subsidiary of The Northern Railway which is now defunct. Any more maple sugar cars out there?



Bob Brown

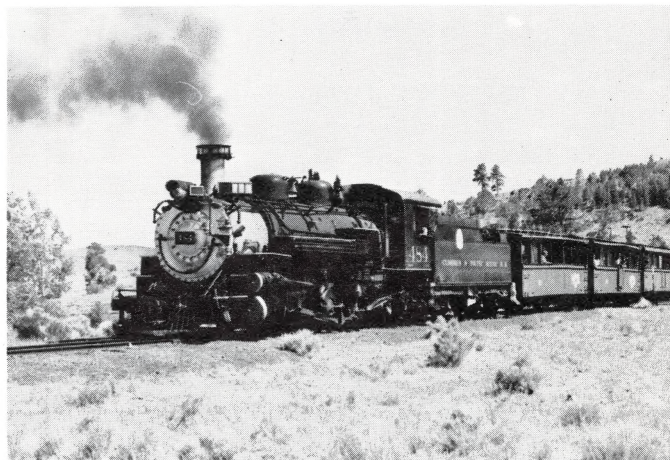
"ON VACATION"



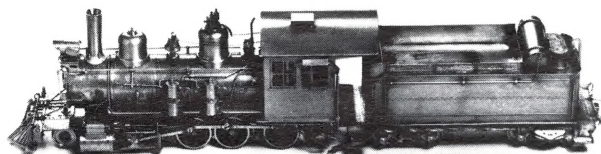
Just to prove we really were in "Narrow Gauge Country" here are three photos taken on our trip. The first photo shows our English guests Daphne and Dick Relph talking to Irene just after a fascinating cab ride on the Silver Plume line at Silver Plume, Colorado. *Photo by Bob Brown.*



The D&RGW's Silverton train bound for Durango in the evening. This train was photographed as it came right through our campground on its way home. *Photo by Bob Brown.*



The Cumbres & Toltec. This photo was taken on the photo-run-by staged on each trip so everyone can get a good photo of the train. *Photo by Bob Brown.*



D&RGW C-16, #223 in Sn3 by PFM - \$395.00

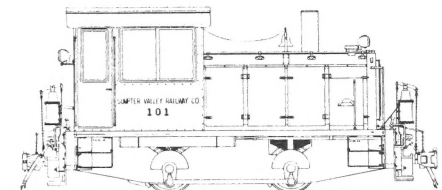
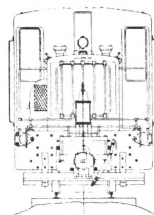
Available late Feb/early March, FLEX-TRACK in Sn3 by Tomalco. #70 n.s. rail in brown Delrin ties of variable length, width & spacing for that authentic narrow gauge look. Closest-to-scale possible "spike heads."



BOX 158
McCRACKEN
KANSAS
67556

D&RGW #50 DIESEL SWITCHER IN Sn3

By the time you read this ad, we expect to have all of the parts for this lil' dude available IN SN3. As soon as possible we plan to offer it as a complete kit, both powered and unpowered. Minutely detailed all-brass construction with steel drivers and axles. Easily converted to On2, could also be converted to On2½. Send long SSAFE for Sn3 catalog sheets and current prices.



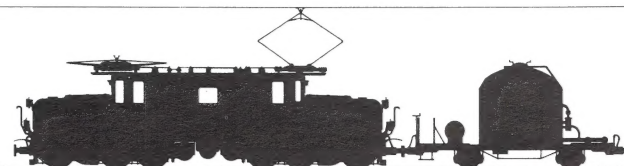
JOHN LEWIS

rittech sa

PO Box 8, CH 1232 Confignon-Geneva, Switzerland (022) 57 20 37

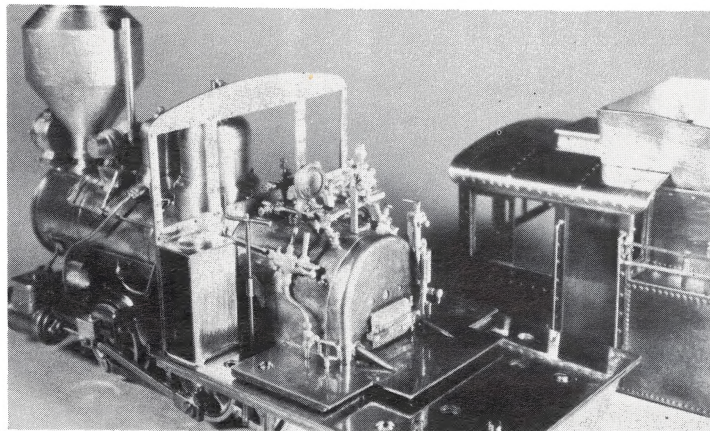
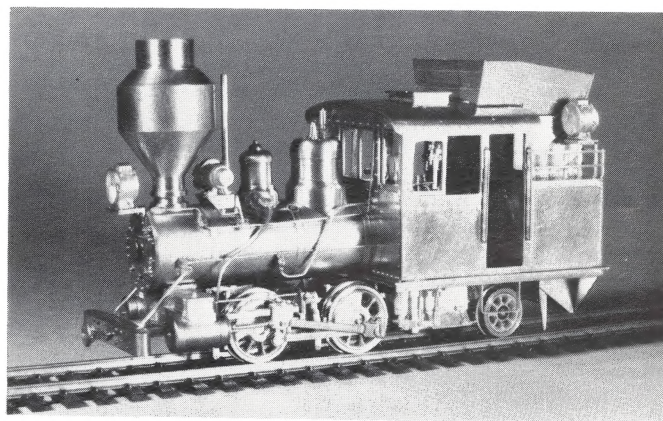
EUROPE'S only supplier devoted almost exclusively to
narrow gauge, branchlines, logging and mining operations.

If it is advertised in the Gazette we probably have it in stock or we will try to get it for you. Fast and reliable mail order service. And we speak your language too: English, Deutsch, Français, Italiano.



SANGO makes your On 30" world

**KISO Logging Railway
BALDWIN 0-4-2T**



Highly detailed lost wax casting
throughout Can motor drive with
idler gear

contact us with some union coupons
for information

SANGO model railroad works.

**2-29-8 Hohnan Sugunami-ku
TOKYO 168 JAPAN.**

(有)珊瑚模型店

東京都杉並区方南2-29-8



The Pigeon Hole

Dear Sir,

Please be advised that Charlie Getz has derailed again. This time he has found a new way to irritate at least some readers with what I believe to be a slap in the face of the D&RGW.

I refer, of course, to the "checklist of attractions" to be found in the Narrow Gauge Scene of the May/June issue. I refer particularly to his comments regarding Chama and Durango. He claims that the Cumbres & Toltec is the only authentic narrow gauge operation in Colorado. Mr. Getz would seem to need to be reminded that the D&RG completed the Silverton branch in 1882 and they are still at it. Train no's. 461, 462, 463, 464, the Silverton Mixed, are still running to this day. Just because they don't carry sheep and ore doesn't mean that the facts have changed. No one is a bigger fan of the C&TS than I am, but I would hardly call what they do authentic. There is only one authentic line in Colorado — the D&RGW.

As if that weren't enough, Getz advises your readers that one day is enough to spend in Durango, since there isn't much to see. And as if that weren't enough, Getz advises your readers to ride the bus back to Durango because it can get lengthy! I find this to be absolutely intolerable for a publication such as yours. If Charlie Getz doesn't like the Durango yard or the "diamond stacks" (which are really cinder catchers to anyone who has ever taken the trouble to find out. I wonder if Charlie would like the C&S beartraps any better, but I doubt it) that's too bad. But it seems like a poor excuse to practically tell people to stay away. I'm sure I don't need to remind you that many of your readers will be coming to Colorado with their families and Durango is a real oasis for the non-railfan members of the group. For those that are, Durango still holds vast new things for everyone from the guy who has never seen three foot gauge to even the greatest authority. All you have to do is look. Besides, leave the wife and kids

for a day while you tour the RGS. I seem to recall Getz rambling at length about that last fall. He must have forgotten.

The D&RGW employees and crews work very hard to provide their passengers with a memorable experience. Many of the old men are veterans of the old narrow gauge and can tell stories of being snowbound at Cumbres, of the Chili Line, of breakdowns and God only knows what else. Remember that picture in Beebe's *Rio Grande* of the 499 that cold, snowy night waiting for an opposing train? The engineman in the picture is the first train's engineer. It doesn't take very long to see that much of what makes the narrow gauge special is more than just the space between the rails. You won't find this type of authenticity in Chama. These guys work the Silverton branch because they enjoy it, and it shows. Another point of interest is that some men are modelers and readers of the **GAZETTE**. Don't you think they deserve better treatment than this? I do. Particularly now that we have to pay more for your magazine. If we have to pay Cadillac prices we won't accept Toyota quality. It's like buying a \$300.00 brass model and finding a wind-up key in the box.

Charlie Getz owes an apology to the D&RGW and to the readers of the **GAZETTE** for not being able to confine his commentary within the bounds of accuracy and the best interests of those vacationers for whom this thing was written.

Sincerely yours,

Michael E. Trent
Boulder, Colorado

No insult to the D&RGW was intended. However you must admit that the paint scheme and K-28 diamond stacks are not historically accurate. Durango is a great town, with friendly people. My remarks were solely aimed at railfans and reflect my personal opinion. For the record I enjoy riding both the D&RGW and the C&TS. Charlie Getz.

Dear Sir:

Can you help me? I'm building an HO_{n3} scratch built model of Colorado & Southern's 4-wheel caboose, #1009. This is the caboose that is on display at the Colorado Railroad Museum in Golden, Colorado. When I was there in 1977 for the NMRA national convention I took outside shots but no interior shots. I really need plans on how the interior was set up. Any and all plans would be helpful. I've sent along a SSAE for your return reply. Oh yes, I do subscribe to your mag. It's really great. Thanks for your help.

Sincerely,

J. Kurilec, Jr.
315 Lombardy Dr.
Berea, Ohio 44017

Dear Sir,

The Art of Weather Wood article was just great. I have been long awaiting to see an article on this subject published. Maybe you can get Marshall "Mic" Greenberg to do an article on weathering styrene and metal parts. I am sure this would be of great interest to all readers. The picture on the cover looks so real, it is hard to tell it is a model. The Gazette is really turning into a very excellent publication. Keep up the good work.

Sincerely,

Clyde H. Queen, Jr.
Alameda, CA

ORDER THIS FABULOUS BOOK NOW

only
\$4.95
postpaid



- ★ 8½"x11" COLOR PHOTO STUDY
- ★ 50 PAGES
- ★ 74 PICTURES (7 Black & White)
- ★ 34 FULL PAGE COLOR

Dealers write on letterhead —
Send check or M.O. direct —

DOUGLAS MODELS

122 East 2nd South
Salt Lake City, Utah 84111

Turnouts ON3

Kits for 3 way WYE, Right to left, code 100, 83, and 70, weathered rail. Split or stub.

On2 Turnouts

Code 70, 83, right & left stub and split turnout kits.

Large SSAE for list.

Calif. Residents—Please add sales tax
ROGER'S LOCOMOTIVE SHOP
P. O. Box 555
Homewood, CA 95718

John's Train Shop, Inc.

Just 5 Blocks North of I-94 • Lexington Exit

100% Model Railroading — We have in stock Narrow Gauge Kits, Decals, and supplies, in On3, Sn3 and HOn3.



THE PIECES AND PARTS SHOP

A Scratch Builder's Delight

523 NORTH LEXINGTON PARKWAY
ST. PAUL, MINNESOTA 55104

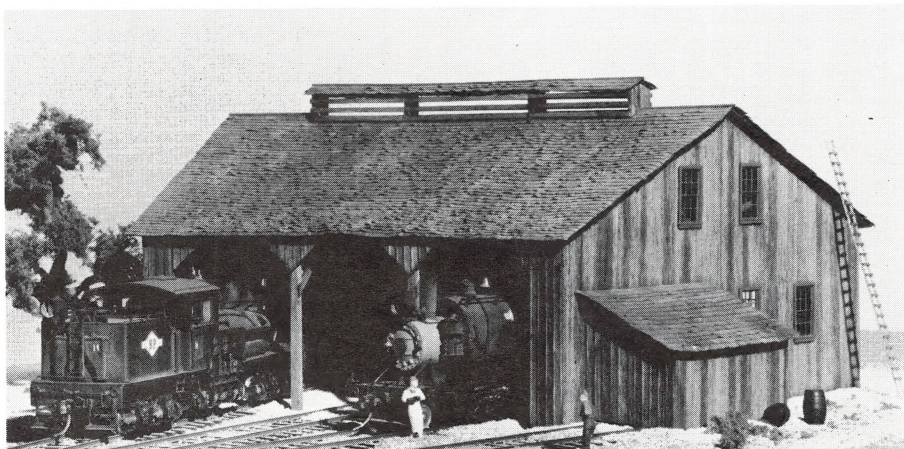


(Area Code) 612- 646-7781

Store Hours

Mon.-Thurs. 9-5
Friday 9-8
Saturday 9-4

HO structure kit



A wood framed 3 stall roundhouse suitable for small HO and HOn3 locomotives up to 50 feet long. Inspired by the Pacific Coast Railway narrow gauge roundhouse at San Luis Obispo, California this is the perfect home for your logging, mining, and branch line locomotives. The completed model occupies a space approximately 11" x 9".

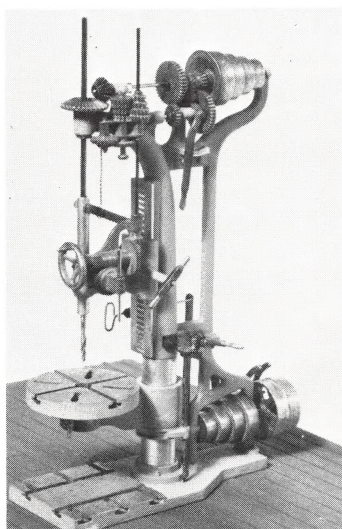
Our kit features KAPPLER die cut milled wood siding, GRANDT LINE plastic windows and stack vents, CAMPBELL shingles, plus complete illustrated instructions and templates. This is a basic kit suitable for modelers experienced in Craft-train construction, and can be superdetailed with the many castings on the market today.

At your dealers today ... \$24.50.

BRANCH LINE ROUNDHOUSE

Kit #7001

**DETAIL
ASSOCIATES**
BOX 187
SANTA MARIA
CA 93456



ANNOUNCING
the second in our series of
"O" scale machine tools

MT-500 36" Vertical Drill
(Prentice Bros. prototype)

\$16.95 at your dealer or direct

C.H.B. Models, P.O. Box 93 Lakehurst, N.J. 08733

DEALERS

MDC-HOn3

2-8-0

IN STOCK!

**TRAINS & TRACK
DISTRIBUTORS**

9052 TERMINAL AVENUE
SKOKIE, ILL. 60076
312/965-3988



Round 'Em Up Pardner For Our On3 Rio Grande Stock Car

☆☆☆ Soon to be at your favorite dealers. ☆☆☆

EMPIRE-MIDLAND MODEL CO.

GPO BOX 1519 NEW YORK NY 10001

GEMINI



FINE QUALITY 1/4" SCALE
BRASS CASTINGS FROM
WESTERN PROTOTYPES.

Send 35¢ for complete list



GEMINI SCALE MODELS

New Address:
P.O. Box 583
Alamo, CA 94507

Motive Power Equipment & Supply

P.O. Box 9 Buffalo, N. Y. 14240
Phone (716) 896-8740

KIT BUILDING IS BACK!

PSC PK-42 K-27 #453 tender kit with dog house On 3 kit . . \$129
K-27 453 Tender assembled by PSC On 3 \$192
At Your Local Dealer or Direct

Narrow Gauge, Standard Gauge,
HO an O. Quality Brass, new and used.
Call or Write your needs.

Master Visa M. O. Terms
N.Y. Residents ad appropriate tax.

**BIGGEST
RAILROAD
STORE IN THE WEST**

Brass Locomotives • Books
1000's of Kits • Details Parts
Structures • Rolling Stock

SPEEDY MAIL ORDERS

ORIGINAL
**Whistle
Stop INC.**

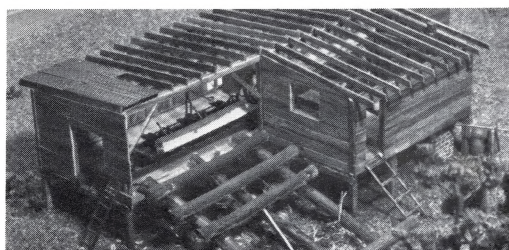
3745 E. Colorado Blvd.
Pasadena, CA 91107

(213) 796-7791

NEW FROM KEYSTONE DANBY SAWMILL

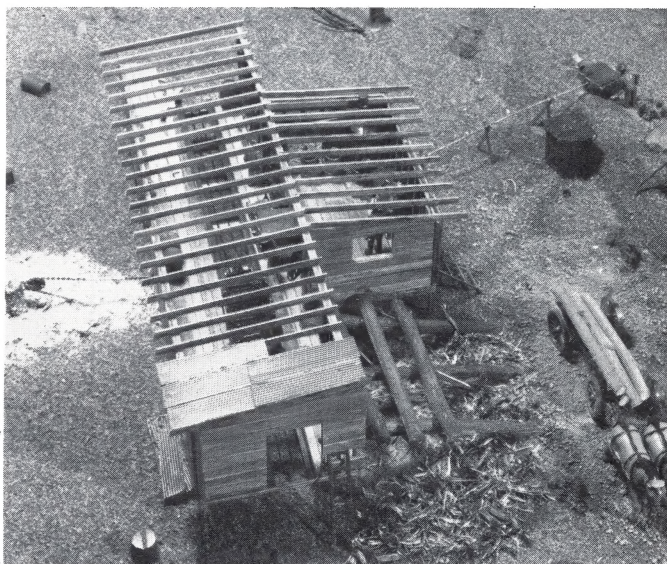
HO-111 \$19.95
O-gauge K-1002 \$37.50

- *BOARD BY BOARD CONSTRUCTION
- *LOGS AND BARK INCLUDED
- *COMPLETE STEP BY STEP PLANS AND INSTRUCTIONS
- *OVER 70 BEAUTIFUL CASTINGS
- *BRASS WIRE AND CHAIN



FOLLOWING ARE INCLUDED:

SINGLE CYLINDER STEAM ENGINE, VERTICAL BOILER, LOGGERS' TOOLS,
FINELY DETAILED LOG CARRIAGE, CORRUGATED METAL ROOF, STONE
FOUNDATION FOR BOILER, CISTERN WATER SUPPLY, outhouse, DRUMS,
BARRELS, BUCKETS, OIL CANS, WORKBENCH, SAWS, TOOLS, MILK CANS,
SCRAP BARRELS, AXES, LADDER, SHOVELS, BROOM, STUMPS.



KEYSTONE LOCOMOTIVE WORKS

159 Wheatley ave.
Northumberland Pa. 17857

CABOOSE HOBBIES

One of the Nations Largest Train Stores

HO BRANCHLINE POWER

WESTSIDE

MA&PA #29 or #30 0-6-0.....	128.88
SP T-1 4-6-0.....	112.00
SIERRA #24 2-8-0.....	117.66
SP P-1 4-6-2.....	105.00
SP S-2 0-6-0T.....	99.95

ALCO

USRA 0-6-0.....	115.00
USRA 0-8-0.....	123.95

PFM

SP T&NO 2-6-0.....	150.00
NP T-1 2-6-2.....	160.00
Long Bell 2-6-2.....	139.95
SP T&NO 2-6-0 cust/paint.....	165.00
Omaha 0-6-0 mint.....	160.00
60 Ton Heisler ptd.....	189.95
Rayonier 2-6-2 #45 cust/paint.....	155.00
WP 4-6-0 mint.....	145.00
GN 4-6-0.....	180.00
SP A-3 4-4-2.....	98.88
UP 2-8-0.....	180.00

NWSL

N&W 2-8-0 W-2.....	169.95
STLSW 4-4-2.....	105.00
Baldwin 0-8-0.....	65.00
Coos Bay 2-8-2T cust/paint.....	155.00
Penn Terminal 2-6-2T painted.....	110.00
Minaret 2-8-2T ptd.....	120.00
Minaret 2-8-2T mint.....	150.00

Serving the Railroad Hobbyist

for over **25** years.

CABOOSE HOBBIES, 610 15th St.
DENVER, COLORADO. 80202

PHONE 303-534-3377



NEW IN "O" SCALE
CHOOCH ENTERPRISES COUNTRY STORE
SPECIAL INTRODUCTORY OFFER.....\$31.95

NEW LOGGING BOOKS

KINSEY - Photographer\$19.95
320 pages, soft cover, A collection of period photos of logging in the Pacific N.W. Excellent Reproduction.
STEAM & THUNDER IN THE TIMBER---Special - \$31.50
By Michael Koch, A Sequel to Koch's Titan of the Timber
Need we say more! (Coming soon-Reserve Yours NOW)

ANNOUNCING: A SECOND PRINTING OF
SWITZERLAND TRAIL OF AMERICA

The complete story of the Denver, Boulder, & Western told in 400 pages with over 350 photos and 4 full color reproductions by Howard Fogg and Richard Ward.
Book due in December. Reserve now for pre-publication price of **\$32.50**

CHARGE CARDS ALWAYS WELCOME: MASTER CHARGE/VISA/AMER. EXPRESS/DINER'S/CARTE BLANCHE
Advise of card number and expiration date. SHIPPING INFO: Exact postage on foreign shipments. NO CHARGE
on Canada & US. orders over \$25. Under \$25 add \$1. PO box shipments 85¢. Furnish st. address if possible.

HOn3 BRASS KITS

KEMTRON

C-16 Orig, Int, Mod.....	107.98
D&RGW/RGS 0-6-0T.....	112.50
42 Foot Flat Car.....	7.99

ADDM

D&RGW 6500 Flat.....	17.95
WSLCo #8 Shay.....	199.00

HOn3 BRASS RTR

PFM

RGS #20 4-6-0 ptd.....	160.00
RGS #20 4-6-0 Mint.....	190.00
RGS #20 Mint w/can Motor.....	200.00
WP&Y 2-8-2 Mint.....	135.00
D&RGW K-28 last run mint.....	225.00
D&RGW K-27 early run (with snow plow & painted) ..	179.95
D&RGW K-27 early run mint.....	190.00
D&RGW K-27 latest run new.....	365.00
D&RGW K-36 2-8-2.....	275.00

BALBOA

D&RGW T-12 4-6-0 c/ptd.....	119.95
D&RGW C-19 2-8-0 Ptd.....	165.00
D&RGW C-21 2-8-0 mint.....	139.95
D&RGW K-36 2-8-2 mint.....	195.00

WESTSIDE

D&RGW C-16 2-8-0 #278.....	135.00
D&RGW C-16 #278 with coreless motor & cust/ptd ..	190.00
D&RGW C-25 2-8-0 NEW.....	125.00
D&RGW K-27 compount 2-8-2.....	120.00
D&RGW K-27 orig. slide vlv.....	150.00
D&RGW K-27 Piston Valve.....	175.00
D&RGW K-28 2-8-2.....	370.00
D&RGW K-36 2-8-2.....	275.00
D&RGW K-37 ptd no box.....	190.00
C&S/RGS #74 2-8-0.....	200.00
SP #9 4-6-0 ptd.....	105.00

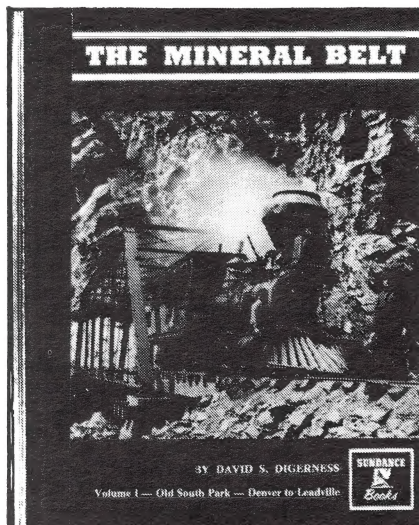
KEY LTD

D&RGW C-18 2-8-0.....	98.00
D&RGW C-19 2-8-0.....	140.00

A NARROW-GAUGE SPECTACULAR!

THE MINERAL BELT

By David S. Digerness and the Editorial Staff of Sundance Publications, Ltd.



OVER 600 ILLUSTRATIONS IN
EACH DELUXE VOLUME!

NEW! ——— Volumes I and II ——— NEW!

The most complete photo-history on the Old South Park narrow-gauge lines—and the supporting mining industry—ever published! Featuring 60 pages of outstanding full-color views, and hundreds of historic 2-color sepia views and black-and-white reproductions—as well as maps, drawings and timetables. 416 pages in each fine volume.

Includes numerous outstanding views of lineside mines, mills, smelters, towns, yards, etc.—in addition to action shots and top-quality DSP&P, DL&G and C&S locomotive portraits. Related Rio Grande views also featured.

THE MINERAL BELT — Volumes I and II
Postpaid in the U.S. and Canada Each **\$45.00**



SUNDANCE PUBLICATIONS *Limited*

P. O. BOX 597 • SILVERTON, COLORADO 81433



Vehicles, scenery details and interior not included.

Lunch break

It's midday and time for a well deserved lunch break for the railroad blacksmith shop crew. Not a soul in sight except for Timmy, the shop's dog. Soon it will be back to work on new main rods for the 278.

The Railroad Blacksmith Shop, based on the Sierra Railroad's structure in Jamestown, is the first offering in our new O scale series called THE SHOPS. Eight Hydrocal castings, including one-piece dormer roof additions, make this building easy to assemble. Both sliding doors open for a clear view of the inside. Grandt details, pre-cut base/foundation, precision basswood, corrugated metal roofing, complete and detailed plans and instructions, plus the usual color photo to aid in painting are included.

O-112 Railroad Blacksmith Shop \$ 44.95 + 2.00 p&h
RETAIL SPECIAL-orders received by Nov. 1, 1978 \$ 39.95 + 2.00 p&h
 THE SHOPS Catalog — new O scale offerings \$ 1.00

California residents please add 6% sales tax.



Thomas A. Yorke Ent.
P.O. Box 34
Sunland, CA 91040

THE E. & B. VALLEY RAILROAD CO. Inc.

0404

NOW AVAILABLE THRU YOUR DEALER*



Build 0404 Or any of these other "Cabeese" from our kit!

Since we are quite positive that the true purist will be more than satisfied with OUR caboose and its attention to detail, we felt that maybe we also should satisfy the narrow gauge modeler who is not too concerned about length, width and minor dimensional data regarding a R.G.S. Vs. D. & R.G.W. Caboose.

*****SO WE ARE PROVIDING IN EACH KIT AN EXTRA SET OF CASTINGS WHICH WILL CHANGE THE CUPOLA FROM A SINGLE WINDOW RIO GRANDE SOUTHERN TO A DUAL WINDOW CUPOLA D&RGW "TYPE" CABOOSE.*****Also included will be instructions on "Kit Bashing" 0404 into a RGS Short or RGW Short caboose and the kit bashing will be a minor matter and easy to do.

- ALL MAJOR PARTS INJECTION MOLDED • CANVAS TEXTURED ROOF
- HO_n3 TRUCKS • ACCEPTS KADEE 714 COUPLERS
- DETAILED UNDERBODY • N.B.W. ENGRAVINGS
- AIRBRAKE ASSEMBLY • ENGRAVED NAIL HEADS

List Price \$ 14.95 ADD 1.00 POSTAGE AND HANDLING. Order direct if not available from your dealer

E. & B. VALLEY RAILROAD CO.


14 Soundview Avenue, White Plains, New York 10606

TEL. 914 946-2657

*This Advertisement was prepared on July 24th and displayed at the NMRA/MRIA Show in Dearborn (July 31st thru August 6th). At that time production was ahead of schedule and delivery should be on schedule.

HO_n3 Passenger Cars Coming Soon!

A NEW SOURCE FOR:
HO_n3 DELRIN TRUCKS
CABOOSE 3'-7" \$3.50 pr.
WITH BRAKESHOES
FREIGHT 3'-7" \$4.00 pr.
WITH BRAKESHOES



THE LITTLE DEPOT


THE NARROW GAUGE SPECIALISTS EXTRAORDINAIRE

**EXPERT
ADVICE FOR
FINE MODEL BUILDING**
MICRO MOTORS and GEAR
HEADS plus most all craftsmen
kits and supplies. WE NOW
ACCEPT MAIL ORDERS!

Drop in for our Fri. nite
Bull Sessions.

OPEN
7 DAYS
MON.-FRI.
10-8
SAT.-SUN.
10-6

THE LITTLE DEPOT
AT HOBBY CITY
DAN and JAN BRADLEY owners
1238A S. Beach Blvd. • Anaheim, Ca. 92804



**BROADEN YOUR
RAILROAD EXPERIENCE**

COME UP TO NARROW GAUGE!

STRIPSTYRENE:
HO SCALE STRIPS - 1x3 to 6x12
S SCALE STRIPS - 1x3 to 4x12
O SCALE STRIPS - 1x2 to 6x12
DIMENSIONAL - .015" x .040" to 1/8" to 1/4"

STYRENE SHEETS:
V-GROOVE SCRIBED-.025" to 3/16" spacing
CAR SIDING-N, HO, S and O - 3/4" spacing
PASSENGER CAR SIDING-HO, S and O
NOVELTY (Drop) SIDING-N, HO, S and O
CLAPBOARD SIDING-.030" to .060" spacing
PLAIN STYRENE SHEETS-.010" to .080" thick

AT YOUR DEALER OR DIRECT
Send long SSAA for info on kits and complete
line of styrene strips and sheets.

evergreen scale models 

1717 N.E. 92nd St. Seattle, WA 98115




Vehicle and scenery details not included.

THE COUNTRY STORE in O SCALE!

The Country Store is the first of our new series of 1/4" scale structures cast in a new rigid urethane. (You can file, scrape and even tap it!) The store comes in four major pieces with a ton of Grandt parts. Easy to assemble! You won't believe the details...right down to the brass door knobs and over 15 signs from the 30's and 40's.

Limited Production. ...so order your kit today! Order our #5109 Coke machine too! It's only \$1.98, and the gas pump is from the Back Shop. Everything else you see comes with the kit!

- Kit #6101, \$34.95 plus \$2.00 p & h
- Available direct or from your dealer.
- Calif. residents add 6% sales tax.
- 1/4" Scale illustrated catalog-50¢. SSAA

Chooch Enterprises BOX 3882, GLENDALE, CA. 91201

COMING SOON

1/4" SCALE

SOUTH PARK — C&S

Gunnison, Colorado Depot

Our first 1/4" scale building, one of the finest ever available! Hand-cast walls in our exclusive dental stone, all custom **GRANDT LINE** windows, doors and roof supports — even the screen door is included — many more special features!

MASTERPIECE SERIES limited production of 250 finest quality models at this time, available by Thanksgiving, 1978. Approximate list price: \$45.00. Reserve now at your favorite hobby shop or direct.

Distributed by Walthers, Train Center, Custom Rly. and A. & L. In Australasia — The Emerald Gazette.

MODEL MASTERPIECES LTD.
P. O. Box 1634
Englewood, Colorado 80110

SILVER SAN JUAN

MODELS

P. O. BOX 186, RIDGWAY, COLORADO 81432



INTRODUCING

Craftsman HO n3 car and
structure kits

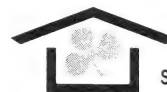
COMING THIS FALL!!!

**CUSTOM PAINTING
NOW!**

Clover House

**TIES * POLES
CHAIN * BARRELS
LUMBER * GLASS**

N-HO-S-O Catalog #2 75¢



Post Office Box 62G
Sebastopol, California 95472

TWO-FOOT CYCLOPEDIA

SANDY RIVER & RANGELEY LAKES RAILROAD MILE BY MILE

VOLUME #4 starts off with a comprehensive history of the SR&RL and its predecessor lines. Then begins the most detailed description ever done for any railroad. Using R-O-W maps; building, bridge and trackwork drawings; coupled with photos and descriptive text, the author gives the modeler and the historian tremendous amounts of facts and details. 200 pages, 11x17 inches, spiral bound. \$27.50 ppd.

VOLUME #5 continues the description of the SR&RL. Volume #4 covered from Farmington to Bigelow including Strong, while Volume #5 covers the line from west of Strong through Phillips to Rangeley and the Eustis branch. This section had loads of logging operations and the mills and log yard are photographically portrayed. 200 pages, 11x17 inches, spiral bound. \$27.50 ppd.

Volumes #4 and #5 are available as a set, and if ordered at the same time you can save some money. The combined offer is both books for \$49.00. The sets are numbered and signed. Only 1,000 sets will be published. Expected delivery date is October 30th, 1978, barring any unforeseen supplier problems or strikes.

SANDY RIVER RR 1879 - 1979 CENTENNIAL CALENDAR

On November 20, 1879 the Sandy River RR laid down its last rail and the first official train entered Phillips, Maine. The 18 mile long railroad was the first two-foot gauge railroad ever built in Maine. To commemorate this historic occasion, FLAG STOP is issuing on November 20th, 1978 a special SANDY RIVER RR CENTENNIAL CALENDAR with illustrations selected from the best of the FLAG STOP collection to capture anew the feeling of the era ushered in by the Sandy River RR. There will be 13 illustrations, including both photos and artwork. Captions are used to further denote special days in the life of the Sandy River RR. Size is 8½" x 11" folded. Paper is 80 lb gloss enameled stock with a bristol cover for added strength when hanging. The illustrations are suitable for framing when the calendar is no longer in use. The calendar can be purchased for \$5.49 and it is mailed via 3rd class. Add \$1.00 extra if you desire first class mailing. A special offer is available for the first 200 (that's 2 feet times 100 years) paid orders. The advance payment for the first 200 buyers gets them a separately printed ink drawing suitable for framing. For those of you who missed out on the first two-foot calendar last year, you had better get your checks off today. This printing is limited to 1,000 copies.

BY SPECIAL REQUEST THROUGH SPECIAL ARRANGEMENT FOR THOSE WHO ASKED FOR IT

We are prepared to offer a limited run set of prints of the ink illustrations done by Tim Pawlikowski for the covers of the TWO-FOOT CYCLOPEDIA, Volumes #4,5 and 6. These prints will include the entire drawing area, portions of which are blocked out on the final cover layouts. The subjects are 1. Sandy River RR #1 and coach in the Farmington covered depot, 2. Phillips & Rangeley #3 entering Phillips as it exits from the covered bridge, and 3. SR&RL engine #24 at Phillips.

The three illustrations are printed on yellow bristol cover stock using brown inks. The size is 11" high by 17" long for each print. There will be ONLY 200 sets printed and each will bear a number. The illustration used on the cover of Volume #2, of SR&RL engine #23, size 8½" x 11", will be included as a bonus since it is work of art which spurred this whole offer.

The complete set comes rolled in a mailing tube at 3rd CLASS rates postpaid for \$8.95. As a special bonus for the first 100 purchases of this set Tim will personally sign the three major works.

Don't delay. This offer is limited to those copies printed in this run and it will NEVER be rerun.

The above listed items can be purchased direct or through one of our special hobby shop dealers. Send SSAE for copy of our dealer directory.

FLAG STOP COON VALLEY, WI 54623

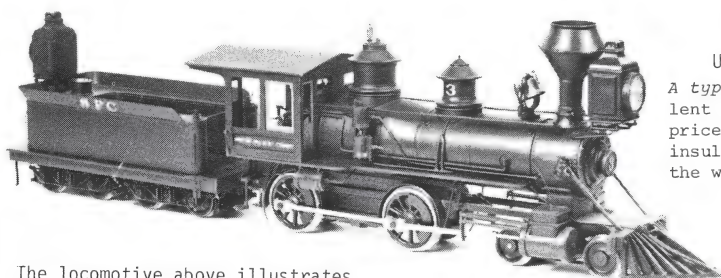


Baldwin On³ 4-4-0 & 2-6-0

SPARTAN SERIES

A CRAFTSMAN'S DREAM 'O' SCALE LOCOMOTIVE FOR UNDER \$200, READY-TO-DETAIL THE WAY YOU WANT IT!

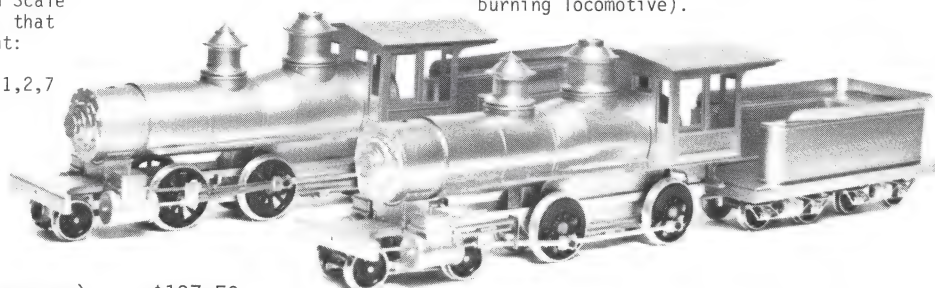
A typical modeler/owner comment: "The model is an excellent starting point for a narrow gauge model... and is priced quite reasonably. It runs well, and the double insulation is a boon to reducing shorts and simplifying the wiring and accessories installation."



The locomotive above illustrates what can be done with the aid of a few Kemtron and Precision Scale castings. A versatile model that can be detailed to represent:

Nevada County Narrow Gauge 1,2,7
South Pacific Coast 2-6
Santa Cruz 3
Sumpter Valley 285,15
D&RGW 85-88, 92-96
Carson & Colorado 1-8

And many more



2-6-0 OR 4-4-0 (BASIC SPARTAN) \$197.50
ABOVE WITH REMOVEABLE CAB 219.50

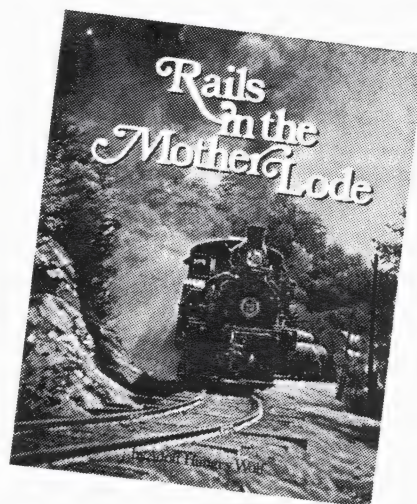
Model is ready-to-run and is available now at your nearest NWSL dealer. Inquire direct if unable to locate.

NorthWest Short Line

BOX 423
SEATTLE, WA. 98111

Rails in the Mother Lode

presents the complete history of the principal lines in the California Mother Lode system - a saga of colorful characters, fascinating equipment and heroic achievement as well as hair-raising wrecks and natural disasters. An absorbing volume, spiced with maps, equipment rosters, nostalgic railroading and nearly 400 historic photographs. More than 200 pages, 8½ x 11", hard bound with full-color dust jacket.



\$16⁹⁵

Calif. residents add 6%

At your book
or hobby dealer
or write direct.

dp
darwin publications
850 N. Hollywood Way - Burbank, California 91505

Peninsula Hobbies

"SPECIALIZING IN CONSIGNMENT"

CONSIGNMENT

U.S. HOBBIES

Pulpwood car \$ 25.00
Operating gondola no box \$ 95.00

CUSTOM BRASS

S-1 \$350.00

PFM

2-6-2 1st run private roadname \$155.00

UNITED

K-27 private roadname \$190.00

NWSL

Weyerhaeuser caboose private name \$35.00
On3 Spartan Series 4-4-0 \$150.00
On3 Spartan Series 2-6-0 \$175.00

SUNSET

Light 2-8-2 \$450.00
(painted/test run only)

MAX GREY

USRA 0-8-0 \$330.00
test run only

CUSTOM BRASS

2-4-4-2 \$800.00
On3 Flanger OD \$ 65.00
Texaco double dome tank \$ 75.00
S-4 \$425.00

SUNSET

2-8-2 Heavy Mike \$525.00

GEM

¼" RDG A-5a 0-4-0 \$250.00

CLASSIC MINIATURES

On3 "Zeroline" tank \$ 28.95
On3 "20 Ft." ore car \$ 8.95

GAZZANO SCALE MODELS

GSM 101 Track Gang (painted) \$20.00
GSM 200 Switchman 4 figures \$10.00
GSM 300 Brakeman 4 figures \$10.00

ASSORTED BOOKS (consignment)

Trains in Transition by Lucius Beebe \$5.00
Highball by Lucius Beebe \$5.00
Highiron by Lucius Beebe \$5.00
Colorado Midland by Morris Cafky best offer over \$150.00
#1634 Mint except for personal inscription
Backwoods Railroad of the West by Richard Steinheimer \$ 10.00

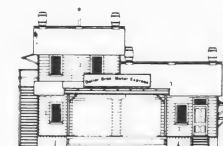


KAPPLER LUMBER SPECIAL

10% off all orders under \$10.00
20% off all orders over \$10.00
any assortment as packaged.

WHEEL WORKS

'34 Ford Pick-up ¼" \$ 19.95



EVERGREEN HILL

HO Ash Drugs \$ 18.95
¼" Dollar Bros. \$ 40.00

Cal. residents add 6% sales tax
Please add 5% shipping on orders over \$15.00
Please add \$1.50 shipping on orders under \$15.00
All items subject to availability, prices subject to change.

415-349-2021

Master Charge, Visa and Bank America cards accepted.

1618 PALM AVE., SAN MATEO, CALIFORNIA 94402

NEW in REVIEW

O/On3 Automatic Scale Couplers

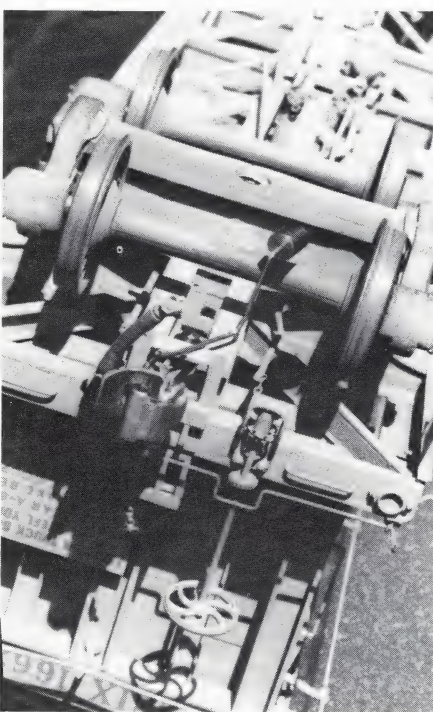
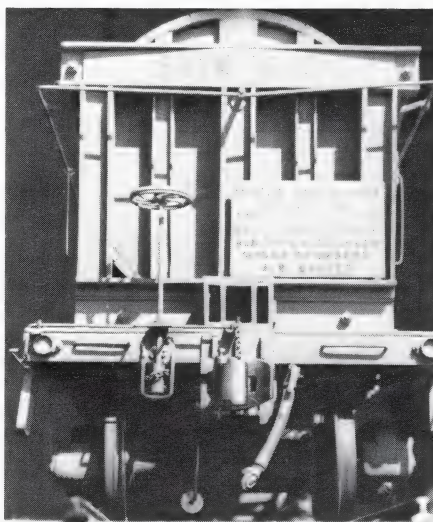
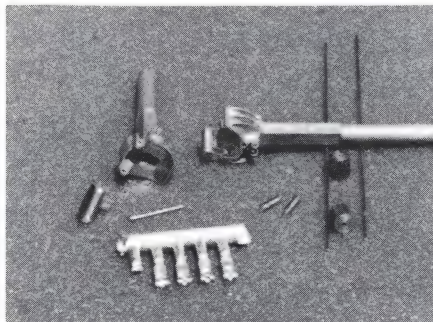
Pacific Fast Mail
P.O. Box 57
Edmonds, WA 98020

At long last, a quarter inch scale coupler is available which is fully automatic. It is the new PFM magnetically activated coupler in kit form. For years scale model builders have had to choose between scale dummy couplers, working scale couplers which did not always couple, or out of scale automatic couplers. Many modelers who spend hundreds of hours on a single car usually end up using the non-working dummy couplers or the unreliable working couplers. Mounting most automatic couplers requires you to compromise the construction of your car underframes. I have used working scale couplers but I find myself having to handle my models while switching due to their unreliability. This often leads to damaged models and little operation.

PFM may have the solution to the problem of prototypical operation with delicate scale models. The couplers operate as well as they look. After a week of testing, I recorded only two failures out of ninety operations. Testing was conducted on straight and 42" radius curved track. I was unable to find out what the minimum operating radius was owing to a limited amount of test track. The **GAZETTE** staff will continue testing the couplers and prepare a detailed article about them in the future.

The couplers are available in kit form only. The kit is comprised of six superb lost wax castings, several types of springs, brass wire, and two steel slugs. The castings are produced in PFM's own foundry. The coupler body, knuckle, and latch castings must be removed from sprues and polished on their working surface. If you follow the excellent instructions provided you should have little trouble completing the polishing phase. Several pivot holes must be reamed out with #76 and #75 drills. The coupler shank must be drilled out for a mounting screw and centering spring.

Most of my cars have brass coupler mounting blocks with a length of 1/16 inch tubing extending to form the coupler pivot. The tubing is tapped to accommodate a 00-90 machine screw. The 00-90 screw and a washer are used to retain the coupler so the tubing provides most of the strength. If you use a similar method, drill the coupler shank with a #50 drill to clear the tubing. Using the machine screw alone won't work, since normal left to right coupler swing will back the screw out.



Assembling the couplers is not difficult except for soldering the latch to the latch pivot. Follow the instructions carefully and it should work the first time. Bending the latch pivot bar takes some time since you must make it fit around the coupler's pocket face and the wheel axle.

The coupler mechanism is quite simple. The key is a part called the latch. This casting fits inside the coupler body and holds the knuckle from rotating open. The latch is spring loaded so that it stays in contact with the knuckle at all times so there is no false uncoupling. To uncouple, the latch must be rotated inward, releasing the knuckle. This upward rotation is accomplished by magnetic force pulling the latch pivot rod towards the track centerline. The pivot rod has a small steel slug attached to it which is attracted to the magnet field. The coupler is designed to work with electromagnetic ramps made by PFM. The coupler will not work with Kadее type ramps since they are polarized on the ends. I have not seen the PFM ramp yet. I used a home made ramp for the testing.

The PFM coupler will couple with almost any dummy or operating coupler on the market. I successfully operated the coupler with Backshop dummy MBC's, Precision dummy and operating MBC's, Clouser Type-E operating couplers, Monarch operating couplers and Kemtron dummies. These couplers will also couple with Kadее O scale couplers but it requires a fair amount of force. For best operation, the interfacing couplers should have a centering spring.

The PFM couplers are very accurate replicas of the AAR Type-E design. Dummy top and bottom operated lift pins are provided on the body castings. Non-operating coupler lift bars may be attached to the PFM coupler but they should not default the centering mechanism.

PFM expects to have these couplers on the market by mid-September. They expect the price to be around \$5.00 per pair. I believe the price is commensurate with the quality of this product. *Gene Deimling.*

Soldering Tweezers

PFM Sound Modifications

Peter-Built Locomotive Works
Box 749
Chama, NM 87520

One of the real treats of a trip to Chama, New Mexico (besides the obvious) is a visit to craftsman Bill Peters' shop to see what is new. Well, my last trip this February showed a lot is new.

First, I obtained a pair of soldering tweezers. Carbon rod or electrode soldering is not new; articles appeared as far back as the November 1954 **MODEL RAILROADER** and November 1964 **RAILROAD MODEL CRAFTSMAN**.

In theory, current passes through the tips and is passed to the part being soldered. Thus the heat is transferred where it will do the most good. I've always wanted to try a pair but didn't know where to obtain this useful tool. Now Bill Peters has made industrial quality soldering tweezers available.

The tweezers are actually not carbon rod, but resistance – actually an advantage as the replaceable metal tips will last longer. The set includes a power control case with five position heat control selector, a foot operated on-off switch, heavy duty cord, and tweezers with formable tips. In operation, one adjusts the heat setting, places the part in place, and zap, it's soldered. It is really that easy and after using one, I cannot imagine how I ever got along without it. While it will not replace the soldering iron or gun, you'll find it extremely useful for big jobs or where a lot of heat is needed fast. All components are of the finest quality, and the price of \$84.95 is very reasonable.

PFM's sound unit is superb in anyone's book. Bill Peters goes it one better with some excellent modifications. He will add momentum to the throttle and add a brake feature. With the brake off, the engine will coast considerably after the throttle is turned off. The brake control, located adjacent to the throttle, increases the resistance. It takes a little getting used to because the brake knob turns counter-clockwise to the throttle's clockwise movement. Also, the brake knob is mounted right through the PFM logo marring the surface. Despite these minor problems, the momentum throttle does add a lot to the operation.

The sound adjustments offered by PBL are dramatic. First, the whistle and bell are adjusted to tone. The most dramatic modification is to the exhaust. It is the most authentic sound you can imagine and improves the unit 100 percent. There's even a "lope" built into the sound so your exhaust will not be unprototypically uniform. (If you plan on running Shays, ask Bill *not* to include this "lope" as it does not sound authentic for geared engines). These modifications list at \$35.00 for the pair.

Bill also offers a modification for a single chime whistle. A small switch is added near the whistle lever allowing selection of the single or four chime whistle. Unfortunately, the placement of this switch on my unit shorted on the transformer housing, and it took a little sleuthing to discover the problem and to solve it with some electrical tape. That single chime does add a lot, especially for narrow gauge and logging engines. It retails at \$28.50, and is a tremendous addition.

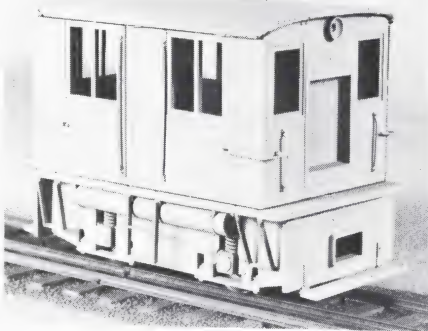
You can get *all* these modifications for \$58.50 plus \$5.00 shipping. These modifications are approved by PFM and are excellent. They make a fine sound system even better, and I highly recommend them. *Charlie Getz.*

25T GE Locomotive

Grandt Line Products
1040 B, Shary Ct.
Concord, CA 94518

When the U.S. Government built the Navajo Dam on the San Juan River it had to relocate some of the Rio Grande's narrow gauge track. The private contractor on this job used a small 25 ton GE Diesel electric loco. The locomotive was coupled to a drop bottom gon on one end and a high side gon on the other. This little train was used to do the ballasting of the new track.

Grandt Line has produced this little loco in styrene in HO_{N3} and HO gauge. The kit falls together and builds into a magnificent little model. Notice the very fine detail in the photo. Especially note the fine handrails. These have to be formed out of wire on a special jig provided in the kit. The locomotive sells for \$8.50 as a dummy unit. A power truck assembly is available for \$45.00. This unit uses a micro-motor and delrin gears. I have seen it operate and it runs beautifully. Here is your chance for some low cost reliable motive power. *Bob Brown.*



HO Branch Line Roundhouse

Detail Associates
Box 197
Santa Maria, CA 93456

Detail Associates has released their first structure kit and it's a beaut. Patterned after the Pacific Coast Railway's narrow gauge round house at San Luis Obispo, California, this one is a natural for HO_{N3} engines or short line motive power.

The kit consists of excellent quality Kappler die cut milled wood siding, Grandt line windows and stack vents, a pre-assembled ladder, Campbell shingles, templates, plans, and instructions. Kappler's wood, as always, is superb. The walls come pre-cut with the window openings die cut. No slivers, no fuzz. Construction is fairly easy although a bit of cutting and fitting is needed for the roof trusses and ventilator. However, the instructions and templates ease the task. Of special note are the full size templates to accurately position the walls.

A speeder shed is also included which also could be used for a machine shop or boiler room. You should also add doors to the stalls as these are not included, if your road is located in cooler climes.

The completed model occupies a space of 11" by 9" and will handle locomotives up to 50 feet. It would look great weathered and cluttered on any narrow gauge or short line road. Kit 7001 retails at \$24.50. *Charlie Getz.*

HO-HO_{N3}-HO_{N30} Turntable Kits

APAG Hobbies, Inc.
P.O. Box 3521
Fullerton, CA 92634

Although enginehouse and round house kits are fairly common, turntables have not been produced in like numbers. Since one is somewhat dependent on the other, this does seem unusual. APAG's new kits help fill that void.

The HO and HO_{N3} kits are quite similar. Both are galleys turntables based on a SP narrow gauge prototype located at Laws, California. The HO version is 65 feet long (8 $\frac{3}{8}$ ") and the HO_{N3} version is 56 feet (7 $\frac{3}{4}$ "). The HO version is also a bit wider to accommodate most small HO engines. Each would look great with the new Detail Associates round house.

The kit is well designed with separate bags containing sub-assemblies. Some of the wood comes pre-cut to length and all is of good quality. Plastic nut-bolt castings, nylon line, and white metal castings are also included. The latter are good quality parts for the bogie wheels and spider mechanism.

Assembly is fairly straightforward due to excellent plans and good quality parts. Some fitting and careful cutting is required, and some experience with wood kit assembly is helpful. There is enough building and fitting here to satisfy any craftsman, and the instructions do assume a certain amount of skill. For example, the drive system is left to the builder, though some helpful suggestions are included.

The HO_{N30} version is not as well designed. It actually is a kit for the superstructure only. Wood, nylon line, and nut-bolt castings are included, but the wood here is not bagged or identified as in the HO-HO_{N3} versions. The style is more complicated; this one is not an "A" frame, but rather a chord style, based on a Sandy River prototype. A bit more fitting is thus required as well as the shaping of some of the wood parts. The instructions are also a bit less complete than the HO-HO_{N3} versions. Finally, no provision for mechanism or suggestions on powering the turntable are included. Notwithstanding those drawbacks, this kit can be made into a very attractive model, and is the only turntable designed expressly for HO_{N30}. That alone makes it worth the modest \$6.99 price. The HO-HO_{N3} kits retail at \$19.95. Definitely, these kits are worth a second look. *Charlie Getz.*

HO Heisler

AHM

415 E. Tioga Street
Philadelphia, PA 19134

AHM's new two-truck Heisler sets new highs in plastic locomotives. I must admit to some skepticism when I first heard of this engine. I expected a lot less than what I got. The detailing on this engine rivals that of brass. The flanges are RP-25, rendering the engine operable on Code-55 rail. Bravo! The mechanism consists of a can motor driving universal joints, much like the prototype, to produce a surprisingly smooth running engine. Even the finish paint is a dull black to kill the plastic sheen.

All is not perfect. Some of the detail parts, such as stamped brass bell and plastic air compressor aren't up to the standards of lost wax casting, but can be replaced. The mechanism, though a mite fast is excellent, but some of the piston rods and valve gear are overscale stampings. Painting and weathering would help hide this. You also might wish to cut back on the length of the truck gear retaining plates for greater realism. Finally, the X-2F coupler should go, to be replaced by Kadees.

In fact, if super-detailed, lubed and run-in for a few hours, and properly weathered, I'd defy you to guess the parentage of the engine. It's that good. In fact, Editor Brown has his eyes on our sample with an On3-conversion in mind! *Charlie Getz.*

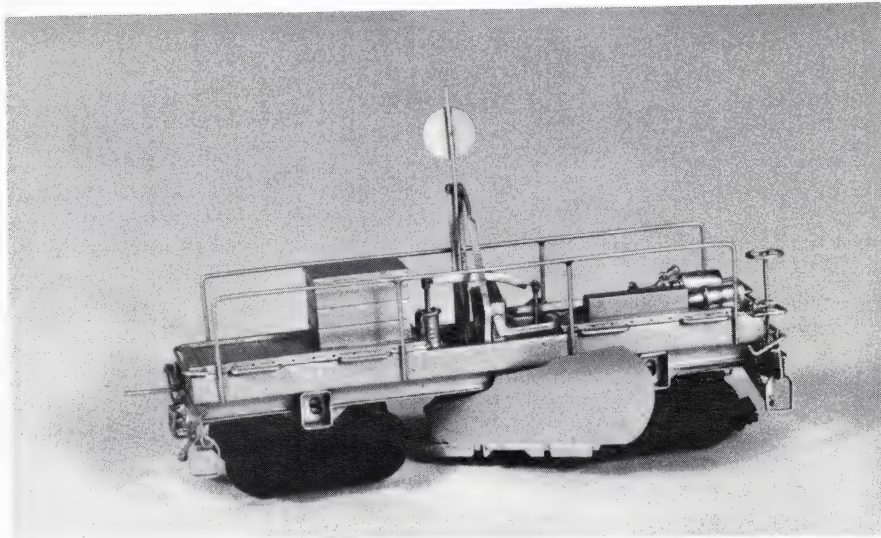
Cedar Shingle Material

Vintage Reproductions

Box 7098

Colorado Springs, CO 80933

Many model builders use thin cedar cigar wrappers for reproducing cedar shingles. The problem has always been getting enough wrappers to shingle a building. If you didn't know several cigar smokers you were out of luck. Now Vintage Reproductions has solved your problem. They now have a packet of rustic cedar available for \$1.00. Each packet contains 8 sheets of .012 thick cedar. Each sheet is about 2½ by 6 inches. The color is similar to that of the weather burnished mine structures found in the Colorado high country. *Bob Brown.*



HOn3-On3 D&RGW Flanger OD

Custom Brass - NJ International

22 West Nicolai St.

Hicksville, NY 11801

Custom brass has brought in one of the smallest brass imports in years—D&RGW's flanger O.D. Only a few inches long in either scale, this flanger is one of eleven once operated by the Rio Grande, no two of which were alike.

CB's model is fairly accurate, but there are a few minor discrepancies. The air cylinders on the deck appear small in relation to a plan published by David Braun. This doesn't mean the size is wrong, as these cars were always in the process of rebuilding due to wrecks. Also the blade drastically limits the truck swing in HOn3. Purists who object to removal of the offending truck's brake beams to allow better operation may wish to park their flanger on a siding.

Overall, this is a most unusual brass import, and a painless way to add a flanger to your MW roster. For prices and availability see your dealer. *Charlie Getz.*



O Scale People

Grandt Line Products

1040 B, Shary Ct.,

Concord, CA 94518

Cliff Grandt's son-in-law, Peter Aduan, has carved two new figures in his series sold by Grandt Line. The figures are cast in white metal and are well detailed. The folds, buttons, and facial details stand out well. I even noticed the button holes are distinct. The little people are designed to be used with the Grandt Line mining loco but would be just as much at home in a passenger car or rail bus. Each figure sells for \$2.00. Painted figures are now available at \$5.00 each. *Bob Brown.*



O Scale Shack

Thomas A. Yorke Ent.
P.O. Box 34
Sunland, CA 91040

I have had a chance to examine samples of the castings used in the Comp'ny Housing kits shown on page 11 of the July/Aug 1978 **GAZETTE**. The idea of using Hydrocal for a wooden building is novel. The photo shows the final effect achieved after coloring these castings. The parts are clean and very sharp in detail. Each nail hole, crack, and split stands out. The kit comes with Grandt window castings and roof material. As shown in Tom's ad the kit is very versatile and can be made into several different buildings. Each shack sells for \$24.95 plus \$1.50 postage and handling. They can also be ordered in sets of three for \$72.00 including postage and handling. Each shack is about 12 x 20 feet. *Bob Brown.*

Loco & Car Plans

Coronado Scale Models
1544 E. Cypress St.
Phoenix, AZ 85006

Coronado Scale Models has several new plans available. These are all large scale plans printed in blue on large sheets. Each plan sells for \$1.50. The first plan is for our little "Coffee grinder" described in last month's Blue Ribbon Series. It is drawn in 3/4 inch scale and is well detailed. This plan is for the Elk River Mill & Lumber Co. No. 1/Falk (Eureka) Humbolt Lumber Co. locomotive. The second plan is for the Bear Harbor Lumber Co. No. 1. This loco was a Dolbeer Gypsy locomotive. The third plan is for a little 0-4-2 saddle tank loco from the Mattole Lumber Co. It was their No. 1. Next is a larger 0-4-0 saddle tanker from the Branch Mint Railroad. It was their No. 1. This line was located in South Dakota. The last two drawings are for cars. The first is for the Rio Grande & Southwestern flat car caboose in three foot gauge. This car also ran on the New Mexico Lumber Co. The last plan is for a three foot gauge cane car built by the Gregg Company and operated by the Oahu Sugar Company. The last two plans are by Paul Eblen and the locomotives were drawn by R. Frick. *Bob Brown.*

Grandt Line Miscellany

Grandt Line Products
1040-B Shary Ct.
Concord, CA 94518

Grandt Line has several new small parts available. First is a set of HO hinges for the side dump gondola described by Irv Schulz in the May/June **GAZETTE** on page 42. This set is #5118 and sells for \$1.00. Next is a new HO scale window. It is double hung, has four panes, and measures 36" x 64". The set sells for \$1.00 for eight windows and is part #5117. Next is a 24-inch diameter cable sheave with bearings. This wheel is also available in 43-inch diameter. This part is "scaleless". It is set #3515 and sells for \$1.00 per pair. A lovely Victorian picket fence is available in both HO and O scale. The HO version includes corner posts and a gate. The O scale set includes about 9 inches of fence. The O scale set is #3514 and sells for \$1.25. The HO set is #5119 and also sells for \$1.25. These fences are hard to scratch built and will be a great help in creating a Victorian scene. *Bob Brown.*

Flash! I have just learned that Grandt Line is about to issue a pair of On3 passenger trucks. Watch the **GAZETTE** for details. Note Grandt's new address above. *Bob Brown.*

Model Masterpieces has pointed out what they feel is an error in our review of their excellent Colorado Midland roundhouse kit. The review appeared on page 19 of the July/Aug. 1978 issue of the **GAZETTE**.

Our review stated that the roundhouse would not fit K-36 and K-37 Class engines due to restricted poor dimensions. We found that the sun shades on a Westside Models K-37 just scraped the sides of the door opening. Any side wiggle would cause the locomotive to hit the roundhouse walls. You could remove the sun shades or mount them at a lower angle to make it fit. We found that our sample roundhouse would clear engines as large as a NWSL Booth Kelly standard gauge 2-6-6-2T with no trouble. *Bob Brown.*

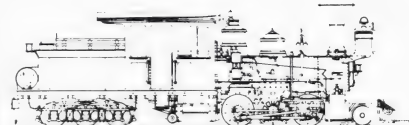
WHERE NARROW GAUGE IS THE STANDARD!

FINESCALER'S DELIGHT!
STYRENE, SHEET & STRIPS
PARTS & SUPPLIES FOR THE
SCRATCHBUILDER!



MODEL RAILROAD
EXCLUSIVELY!

1610 JEFFERSON □ WATERLOO, IOWA 50702



P.F.M.-WESTSIDE-LAMBERT

HO-HOn3 • NEW - USED

LITTLE TOWN HOBBIES

118 WOODLAWN CTR.
LITTLETON, COLO. 80120

DEER CREEK LOCOMOTIVE WORKS

PROFESSIONAL

Custom Painting

SSAE For Info



P.O. Box 3496 Arlington, Texas 76010

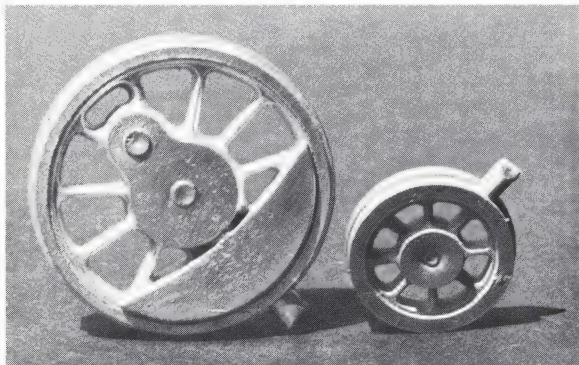
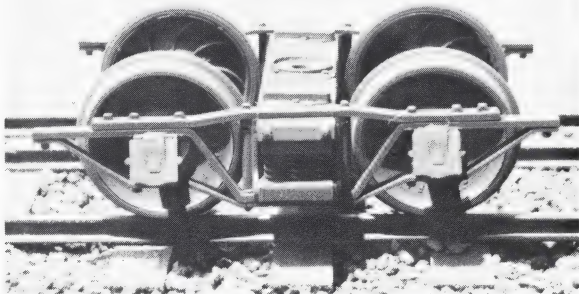
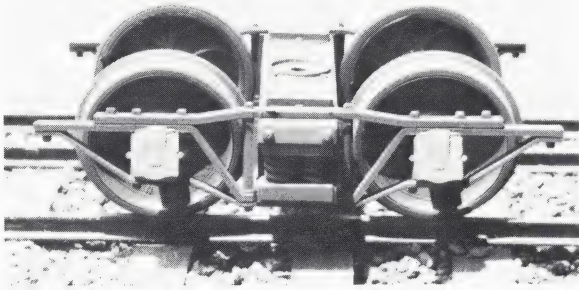
THE GOLD SPIKE TRAINS



WSM D&RGW K-28 2-8-2	\$350.00
WSM D&RGW C-16 268	\$120.00
WSM D&RGW C-16 278 Mint	\$120.00
WSM C-1 Three Truck Shay	\$290.00
Key D&RGW M-68 4-8-4	\$220.00
CB D&RGW M-78 4-8-2	\$200.00
RGM D&RGW Pile Driver OB	\$31.00

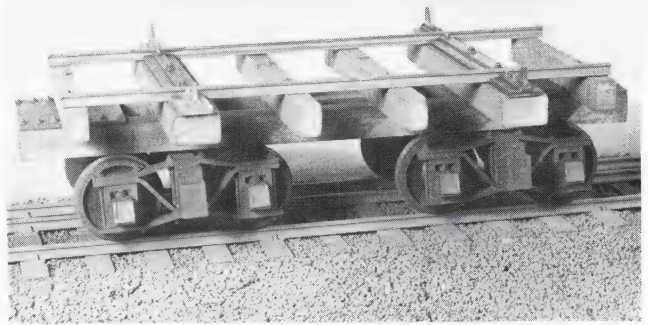
Send SSAE for Newsletter

1005 PLYMOUTH DRIVE
COLUMBIA, MISSOURI 65201
PH. (314) 445-4030

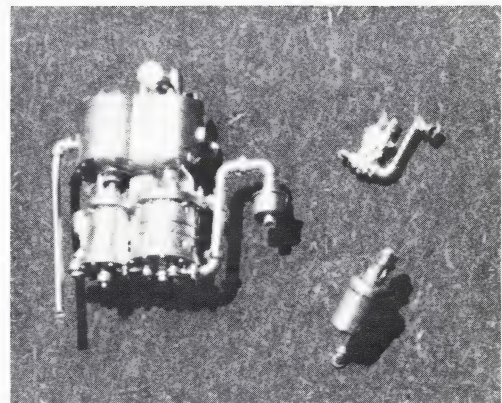


MacLeod Western, 22 Ivy Drive, Orinda, CA 94563, has just released two new quarter inch scale three foot gauge plastic truck kits. Both trucks follow the Westside Lumber Company type III-B 3'7" wheel-base design. The two trucks differ in the number of dummy springs which are seen from the side view. Kit #T-11 is for the 3 spring version and #T-12 is for the 2 spring type. Both kits sell for \$3.00 per pair less wheels. All the parts are well formed and appear to be very accurate renderings of this prototype. They can be used under a number of the Westside cabooses, tanks, and work cars.

MacLeod also has released two new lost-plastic brass castings for quarter inch scale. They are the Baldwin driver center for 48" diameter wheels (N-1011) and the spoked center for a 28" lead-trailing truck (N-1012). The parts are intended for use on the Little River #126 2-4-4-2 but can be used on a great number of post-1900 Baldwin locomotives. The castings have a clean surface and are wide enough to use with NMRA standard wheel profiles. The price for N-1011 is 2/\$3.00. N-1012 is 2/\$2.45. *EED*.

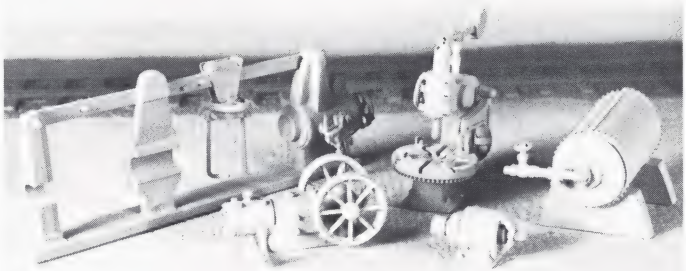


Keystone Locomotive Works, 159 Wheatley Ave., North-unberland, PA 17857, has brought out a new kit to go along with their O scale Grasse River Caboose reviewed in the July issue of **THE GAZETTE**. This new kit consists of a pair of Grasse River log buggies. The kits are composed of good quality wood parts which are all cut to size. White metal castings are provided for the bunks, chocks, nuts and bolts, brake cylinder and brake wheel. Brass rail is provided for the loading rails which run along the top of the cars. The kit comes with special Russell pattern trucks with O scale wheels. The cars do not come with couplers. I assembled one of my cars in about three hours. The car went together easily and all the parts fitted. I painted the metal parts before assembly and pre-stained all of the wood. These cars look great with the caboose and are short enough so a train of them will not be too long. The kit sells for \$17.95 for two cars. *RWB*.

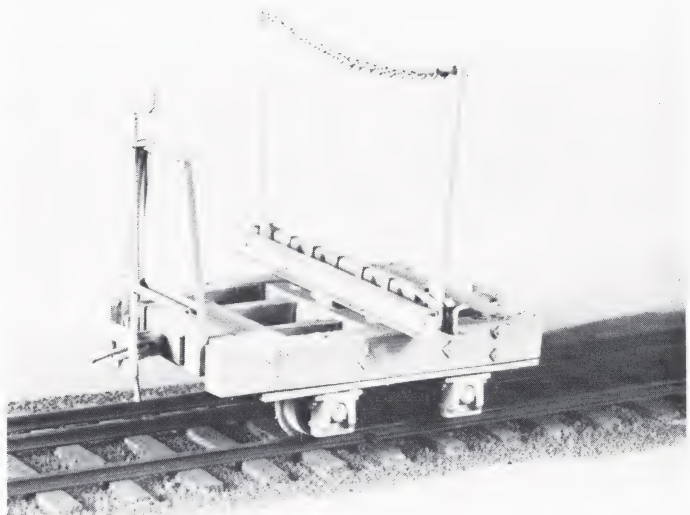


Precision Scale Company, Route 1, Box 1802, Davis, CA 95616, has released a new HO scale Westinghouse air compressor in lost-plastic brass. The part is an accurate model of the familiar cross-compound used on a great number of locomotives. The pump casting comes with an AD governor and a choice of two different styles of air cleaners. Ask for #3092-2. No price available. *EED*.

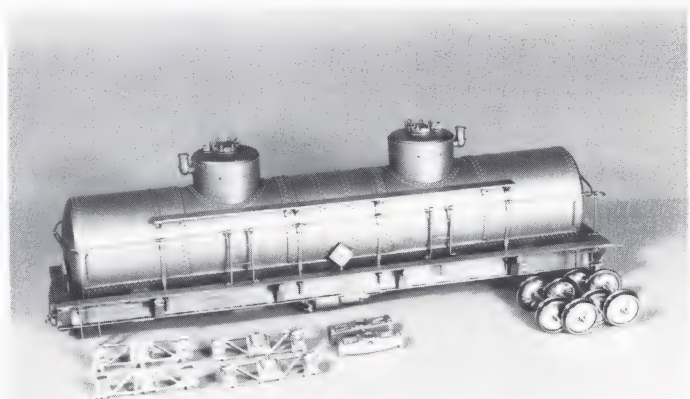
Rio Grande Models, Box 4463, Santa Clara, CA 95054, has two new shop tool kits available. Both of these kits are composed of white metal castings and are well detailed. Both of mine went together in about a half hour. The parts are well formed and fit together with no trouble. The first set is for an air compressor and tank. This set consists of an electric motor, compressor with a pulley wheel and flywheel, and a large riveted tank with concrete bases. Several pipes are also provided. The second kit will provide you with a vertical wheel borer and a wheel press. The borer is like a vertical mill and the press is a rather long frame for holding an axle while the wheel is pressed into place. See your dealer for these fine detail pieces. *RWB.*



This little On30 log car is available from Sango Model Railroad Works, 2-29-8 Hohnan Suginsmi-ku, Tokyo 168, Japan. The cars come in pairs and sell for 4600 yen. Check with your local bank since the yen is going up all the time. Or is the dollar going down? Each car consists of a white metal main body casting to which brass stampings are added. The parts fit well and the detail is excellent. The wheels are very small and the link and pin couplers are well detailed. In fact, there are two styles of link and pin couplers on each car. *RWB.*



This D&RGW TEXACO tank car by ADDM is a beauty. The car comes assembled except for the trucks and couplers. The body is a well detailed and neatly assembled unit. You merely have to file off some sprues on the side frames and assemble the trucks. The wheels used on this car are brass castings with steel tires, the centers are fully detailed and have the raised lettering on both the front and back. These cars will be discussed in part II of our tank car series. Briefly, there were four of these two dome cars on the D&RGW. They were not owned by the railroad but by the Texas Co. The cars were numbers 70, 71, 72, and 73. These were the only two dome tank cars to be used on the Colorado narrow gauges. Write to ADDM, Rt. 1, Box 1802, Davis, CA 95616 or see your dealer to find one of these cars. *RWB.*



The Narrow Gauge SCENE

by Charles Getz



September mornings break clear and cool – at least in most parts of the country. Here in San Francisco, we start our “summer” in September – a few weeks of warmth to compensate for the foggy coolness of the past months. Of course, September marks the traditional commencement of the modeling season, and a good time to get cracking on your next project. Before I get onto my project – the news – let me thank Dale Fleming for the new title headings – nice!

A few weeks ago, I had the great opportunity of visiting that “other” narrow gauge, the East Broad Top. Accompanied by author Gordon North and his son Garry (How many other railfan trips have a sound man along?), we toured Orbisonia, Mt. Union, and a few other legendary spots. All in all I was quite impressed by the EBT. It’s like a railfan’s dream come true, for here is a whole narrow gauge line, preserved, as if frozen in a time warp. The shops are complete with tools and machinery, just as they were left in 1956. The yards are full of hopper cars awaiting trains which will never come. Even the main line has rail throughout, though at places the track is wholly engulfed by the terrain. Mt. Union’s yard, replete with 3-rail track, still has a standard gauge 0-6-0T safely sheltered inside the enginehouse.

You tour Orbisonia in a dream, as if this marvelous anachronism is going to disappear in a puff of smoke. It is comparable to what it must have been like to tour the RGS in 1952 or the Rio Grande in 1954. It is also a happy anachronism, for the EBT still runs. Not coal, of course, but passengers are carried over about five miles of its length. Unfortunately, it wasn’t running when I visited, but I’ve vowed to return when it is.

The EBT is certainly worth modeling. While the scenery is not the Rockies, it can be effectively modeled using the techniques of such lichen belt railroads as the Virginian and Ohio. Its equipment is distinctive and varied, and the Orbisonia yards were apparently designed with the modeler in mind. If you don’t model the EBT, at least visit it. No railroad should be taken for granted and that goes double for a narrow gauge line. There is no

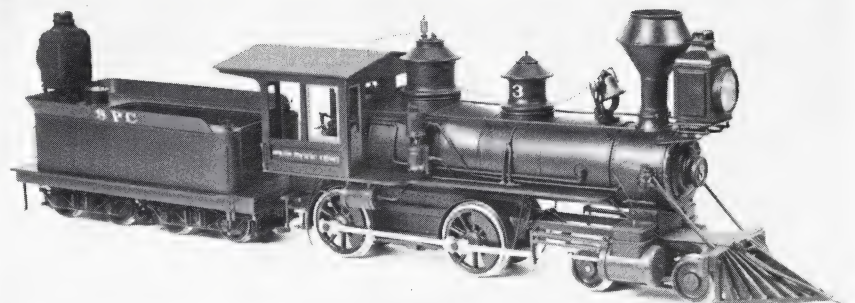
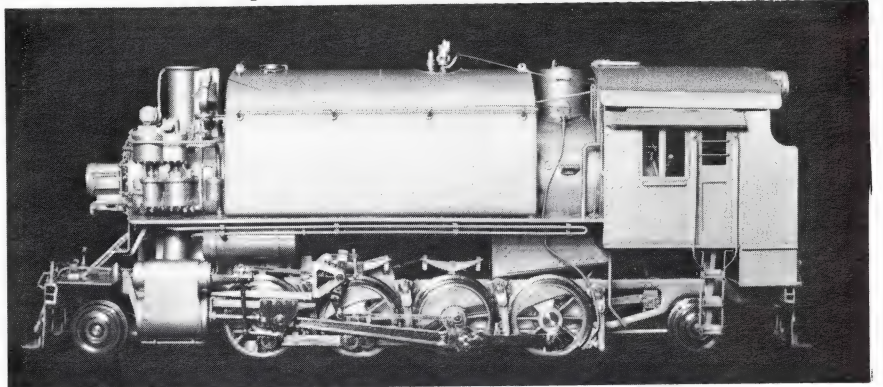
guarantee that this operating piece of history will keep operating without your support. Ride it, tell your friends about it, get concerned. Preserve it on film, for one day it may be gone and then it will be too late. It runs weekends in June, September and October. The line runs every day during July and August. If you have missed all of this your next chance to visit the EBT is their spectacular held in February. If triple-headed narrow gauge steam doesn’t excite you, forget it! You’ll find information on the EBT in the August, 1978 **RAILROAD MODELER**, Winter, 1975 **RAILFAN**, and the December, 1971 **MODEL RAILROADER**. Quality Craft produces an EBT hopper in On3-HOn3 and has offered the caboose and box car in the past. EBT mikes may be available soon in both scales.

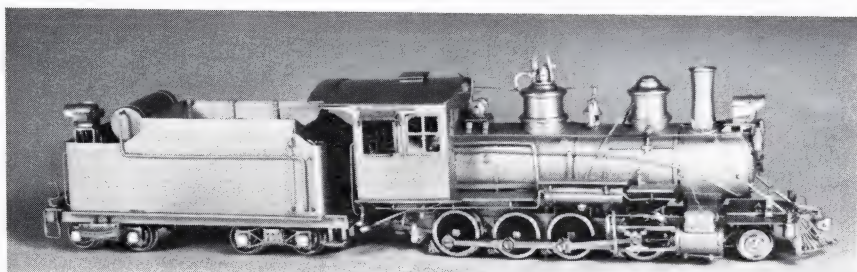
Speaking of engines, brass is slowing a bit – at least in narrow gauge. No recent news from PFM. Westside’s new C&S/RGS #74 in HOn3 recently came in and looks good. That’s what you know. What you *don’t* know is that the entire shipment arrived with the wrong cinder-catcher

stack soldered onto the stack. Rather than let this go by, Dick Truesdale, to his credit, replaced all the boilers with new ones with correct stacks (not soldered).

The Westside’s K-28 is a tremendous job, with superb detailing, including the much-touted rounded cab corners. I guess the best compliment is that it just looks right. Nakamura, the seasoned Japanese builder, shows his long years of experience in the building of this engine. We hope to secure one for review soon to really put it under the scope. You do the same at your local shop.

NorthWest Short Line sent some photos of their products, taken by Scott Law. Their O scale Minaret is quite impressive, with a list price of \$784. The prototype ran on the Hammond Lumber Company as their #17. The model has not only a fully detailed cab interior, but also a fully detailed boiler under the removable saddle tank. Another nice touch is a working lubricator linkage. The model is powered by a large Sagami can motor through an enclosed gear box. Minimum radius is 24 inches. The second photo is of





an On3 Spartan Series 4-4-0 detailed to represent South Pacific Coast #3 by Jeff Richardson.

NWSL's new tiny Shay, CN #60 (that's construction number 60) should be available now. List about \$250. Other tiny Shays to come. One disturbing note – the excellent "Flea" units are discontinued, effective immediately. Apparently, there was just too much hand-labor involved to make production feasible. NWSL has promised a replacement unit soon. Let's hope so, for the Flea enabled a lot of unpowered rail cars to be powered.

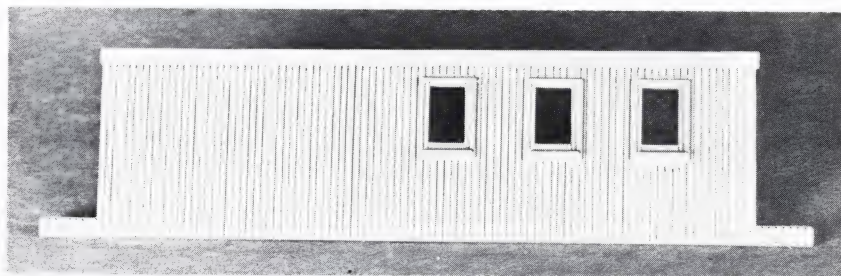
After a long absence, the C-19 is again on the market in HO_{N3}. Once offered by Balboa, it is now being imported by Key, based on D&RGW #346. This engine was built by Baldwin in 1881 with a tractive effort of 18,950 lbs. Technically a C-18.95 (nitpickers arise!), C-19's were used on the RGS, and on the C&S under lease from the Rio Grande. Number 346 was used on the Rio Grande until May, 1947, when it was obtained by the Montezuma Lumber Company and eventually preserved at the Colorado Railroad Museum. It is still in service today.

Key's model is a fairly good representation of the C-19 class consolidation. While numbered 346, it appears closer in detail to 345. One unusual feature is a stepped running board with air tank on the engineer's side. While Key states that their model is correct for 1930-40, photos of the prototype in this era do not show this configuration, but rather a straight running board with no air tank. Also, sand lines extend back from the dome differently than in pictures I've seen. The domes are also different, reflecting their current configuration. Otherwise, the detailing is excellent, including a full cab interior in the "deckless" cab. One problem with the tender should be noted. Both the 346 and C-18 #318 are at the Colorado Railroad Museum. Apparently the day that the Key crew was out measuring #346 and #318, the museum staff had "swapped tenders", so #318 could be steamed. Yep, you guessed it! The tenders got reversed on the models so if you own both C-18 and C-19, simply reverse your tenders!

The model is powered by a can motor and runs very smoothly. Starting speed was on the high side, and it runs noisily, but a bit of lubrication and adjustment should help. For some reason, mine ran better forward than in reverse. This is

a fine good-running engine, well worth the price asked. I, for one, am glad to see it back.

On the rolling stock front, the big news is still the RGS Rotary in On3 from Durango Press. While the price has attracted a certain attention, its very existence, high state of detailing, and limited availability have certainly attracted more attention! In fact, sales of this once-in-a-



lifetime model are quite brisk. If you have ever tried to scratchbuild a rotary, you'll understand why. Don't be sorry later by not ordering one now. We have one, are examining it, and a review will appear just as soon as space allows. By the way DP has moved to Seattle. Effective Sept. 1, their new address is P.O. Box 1836, Lynnwood, WA 98036. That address is also good for the Wheel Works.

On the other end of the RGS size spectrum, E&B Valley Railroad has finally released its plastic HO_{N3} caboose. RGS 0404 is here! At deadline, we had only seen a side casting, shown, and we're encouraged. The detailing is quite acceptable, and owner Bill Glass advises that the final run will be much better detailed. Let's hope that RGS 0404 sells well enough to allow production of the other goodies the folks at E&B have planned. For example, E&B will shortly release caboose and freight trucks in HO_{N3}. The big news however is passenger cars a la D&RGW in HO_{N3}! Bravo! More news on this later.

Sandy River Car Shops, 567 Rockdale Ave., New Bedford, MA 02740, sent a new listing of products recently. New releases include a Bridgton and Saco River box car #65 and flat car #8 in On2, as well as box #56 in HO_{N2½}. Portland Products, available from Sandy River, has some new brass etchings in On2 including SR&RL #6 (0-4-4T Portland Forney) #8 (a2-4-4T) and #1-2 (Hinkleys). Prices and availability of course vary, so drop a line with SSAE for information. You might also be interested in photos

available from SRCS. These cover a variety of two-foot lines and all prints are 5 x 7 modestly priced at \$1. Some of these date from the 1920's, so if you are a two-foot fan, you'll want to obtain this list. Write for the list, and don't forget the dry transfers mentioned last issue.

Prolific Eric Bracher of Rio Grande Models keeps producing all sorts of nifty items. In Sn3 the list is growing. First, tender wrappers for C-16's #268 and 271 are available. Number 268 is priced at \$12.95 and is made of etched brass. Also new in Sn3 is a Swayne log car and Pile Driver OB. Despite its name, hinting of a medicinal function, a "pile driver" is in actuality a large drop-hammer to force pilings into the mud in bridge construction. Anyway, the Sn3 kit is really a hardware kit with all the needed fittings, trucks, plans, decals, and instructions. Price is

\$19.95. Upcoming also in Sn3 is D&RGW water car #0470.

RGM also has the three-way coupler car complete with trucks and decals available for your HO switcher crews. Also in HO is a new light overhead traveling crane such as is used at the Sierra's Jamestown machine shop. A medium size crane is also planned. Finally in HO_{N3}, Eric is working on D&RGW derrick OP with idler car. What, is that all?

Simpson sent disturbing news. Due to the press of outside interests, Simpson has decided to discontinue the entire Slim Gauge Productions kit line, most of which will never be offered. Just structure and car kits will be discontinued; *not* track, strap stock, wire, decals, track gauges or plan packs. Occasionally, a new kit may be produced, but there are no new releases planned. A few of the new On3 West Side Lumber Company wooden water tank cars are still available. If you want the On3 RGS 0404 or shorty D&RGW caboose 0575, or the HO Allison depot, Jack's Cabin water tank, or any of Russ' other distinctive offerings, *now* is the time to order. Unfortunately, there will be no tomorrow.

In our abbreviated Products of Interest department, Detail Associates has some new HO freight car detail parts available. Also new are N scale parts including logging equipment such as a steam donkey and log bunk. New in HO is an extra stall kit to match their excellent branch line roundhouse ... Simpson's newest plans include the massive West Side lumber mill (\$5.95) and Dolbeer donkey plans

EVERGREEN HILL designs

SHENANDOAH CROSSING

EHD-201 c contents

2 oil high boys	1 vise
3 bulk oil cans	1 grinder
1 tire pump	6 tires
6 oil bottles	1 water cooler
1 motor block	1 hat rack
3 mufflers	2 chairs
6 industrial lamps	1 pile of sacks
5 industrial lamps	1 pile of 18 sacks
1 compressor	1 open sack
1 tiny tools	1 shovel
2 pails	2 spade
1 push broom	1 coal hod
1 straight broom	1 box filled
1 mop	2 boxes empty
2 oil drums	1 manure fork
1 desk	1 hay fork
1 swivel chair	1 hoe
1 wall phone	1 scythe handle
1 acet. bottle	1 hand axe
1 oxygen bottle	1 cooper axe
1 welding hose	1 post hole digger
1 lug wrench	1 scythe
2 trash cans	1 bottles and jars
1 pot belly stoves	1 shelves
1 toilet	1 sales counter
1 sink	1 P. O. boxes
1 cash register and paper roll	

HO

ATTN. owners of EHD-201b kits. Your interior coupon is good until Oct. 6th, 1978. If you wish to purchase an interior kit send your coupon and remittance prior to this date. **AFTER Oct. 6th, 1978** these interior kits will be sold on a **first come first serve** basis. Cost: \$20.00 + .75 for s & h and \$1.20 tax for California residents. Only 90 left at this time.

Evergreen Hill Designs
P.O. Box 396
Belmont, California 94002

(\$6.50). Superb and accurate as always – more plans coming ... Fine Scale Miniatures next super-duper release will be a railcar shed. Only 1500 kits will be produced, not 3000 as in the past, so those yellow boxes ought to be as scarce as hens' teeth. George Sellios promises ever finer detail ... Keystone's excellent O scale sawmill, reviewed in the May issue is also available in HO. Believe it or not, the detail is *just* as good ... Chooch Enterprises is branching into the structure field with a polyurethane foam building kit in O; the first will be a store for about \$34.95. If you recall Mike O'Connell's Circa Thirty diorama in MR some years back, you can anticipate *quite* a kit since Mike is part of Chooch.

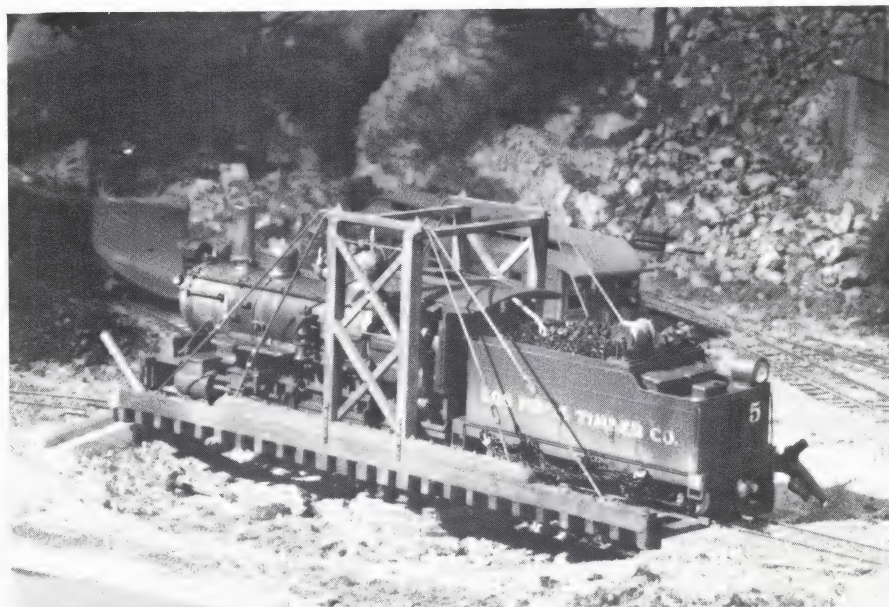
S.S. Ltd. has finally released its Ponderosa Pines, and they are truly artistic. Sculptured is the only way to describe the cast trunks, and a few of these super detailed giants will enhance any forest. S.S. Ltd. is also re-releasing their boiler house in corrugated, stone, or brick versions. You can select your style, and an interior is included with three large boilers! Also re-released will be the oil pump, oil tanks, and unloading facility so necessary to you petroleum czars. The biggest news from Scale Structures is actually small – a new series of detail castings to dress up an HO model. Architectural details such as tenement chimneys (4 styles), firehouse bell, porch fan bracket, mailbox cluster, and city tree base supports are just a few of the incredibly detailed castings available. Over 200 new castings have been added to S.S. Ltd.'s extensive line, and all are illustrated in their new metal casting catalog which retails for \$2. If you are a detail hound like me, you'll appreciate this most complete line ...

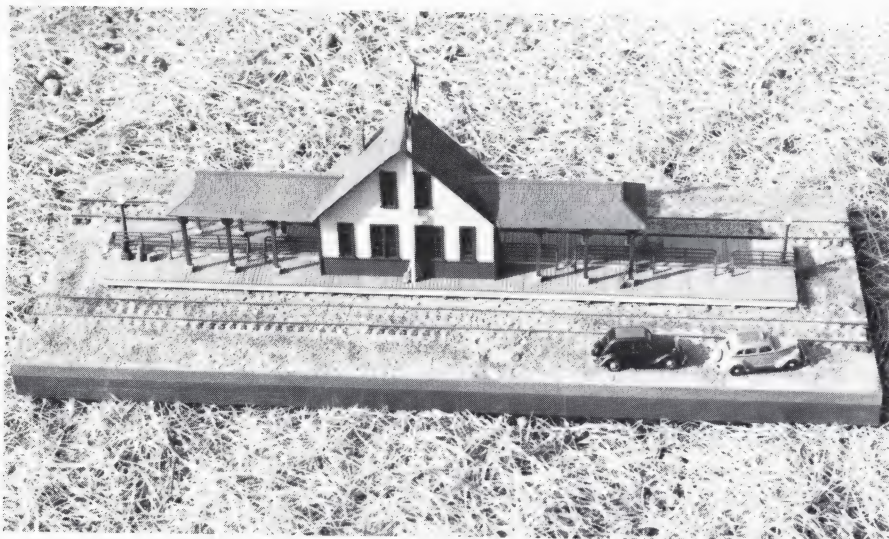
Sugar Pine Models is back on the market after a period of inactivity with eight new kits. Included are a farm-type

covered water tank, a branch-line water tank, sand house, and logging enginehouse. A new flyer showing these kits is available for an SSAE. Welcome back ... Gary L. McNeal Hobby Shop (28 West Third Street, Lewistown, PA 17044) submitted samples of their excellent East Broad Top herald decals in HO n3. They're available at 40¢ a sheet with at least four heralds per sheet. Include a 15¢ stamp for postage. On3 is expected later ... New from Irv Schulz's "125 Line" is multicolor sets for Heinz pickle cars. Some cars have up to six colors, and a variety of products are offered such as 57 Varieties, Pure Malt Vinegar, "Pickles" vinegar car, and Preserved Fruits. (I'm *not* going to touch that line!) They are available in HO at \$1.40 per set, and an SSAE brings a price list. Now your layout can be properly pickled (ouch) ...

The Structure Co. is releasing some new buildings and that is pretty exciting. New is the Montezuma, Colorado, post office and Grand Central gold mine ... Walther's new Beer Can tank car is now available in HO as well as O. A fun item for those who don't take their hobby too seriously. Who'll be the first to narrow gauge one? Model Masterpieces new D&RGW-RGW turntable kit is out, as shown in the photo. The Como Colorado turntable designed to match MM's Como roundhouse should be available soon. By the way the engine on the turntable is a Model Die Casting 2-8-0 superdetailed by Forrest Van Schwartz. Enginehouse and ash pit mentioned last issue should be out soon, as should be the ¼ inch scale Gunnison depot. The latter should be really nice with new Grandt castings, and even the screen door! Priced at about \$40.00.

I've just assembled what may be Timberline's last offering, Franklin Jct. depot and it's pretty decent. It's designed as the





perfect depot for transferring passengers from standard gauge to narrow gauge and vice versa. Mine will perform this duty on the HOn30 Eureka Northern. The kit has quality wood (some pre-cut), Grandt windows and doors, and average quality white metal castings. You might look into this one before it slips away. Available from Walthers or Pro-Custom in Denver at \$19.95.

Thinfilm Decals has some new sets out. That's good news for all narrow gauge fans, especially since they are in both HOn3 and On3. Newly released are CONX 10-11, CONX 5, Texaco two-dome tank cars, and D&RGW 6500 series flat. All available at \$1 each. (TCX-CONX-CYCX will one day be offered on a large sheet.) These small sheets letter one or two cars and are the Cadillac of decals. Order direct from Thinfilm, P.O. Box 444, Canoga Park CA 91305.

In the book nook Mallory Hope Ferrell has not let any grass grow under his byline. Beside his *West Side ... Narrow Gauge in the Sierras* penned for PFM – and due momentarily – Mal has just finished writing the newest *Colorado Rail Annual* on the Utah and Northern, once the nation's largest narrow gauge mainline. If that isn't enough, he's busy on a new C&S book covering that road's final decade of operation. It will contain 200 pages with about 400 photos, plans (C&S #74, business car, reefer, box, gon, 4-wheel caboose), and two fine paintings by Jan Rons. Look for the dust jacket showing double headed #61 and #58 circa 1936. Mal cranks out books like most of us do kits!

If you like Mic Greenberg's weathering article last issue, you'll love the new booklet series he is co-authoring with Gary Nash. It will cover the topic from dust to rust in multi-volume pamphlet format for \$3-\$5 a copy. Look for it soon.

In the Musical Chairs Department, Paul Scoles (formerly of Kemtron) has joined Durango Press. Brian (Bullwinkle) Leppert (formerly of Durango Press) has joined Bill Gould. To complete the circle,

John Hermann (formerly of S.S. Ltd.) has joined Kemtron. Who's on first?

In the Help Department, John Benson, 1561 - 16th Avenue, Kenosha, WI 53140, will offer some excellent new narrow gauge plans soon. You'll be seeing some of John's superb efforts in the **GAZETTE** through his generosity. One project is 3/4 inch to 1 foot drawings of the K-28, and John needs some help on details. He is looking for good close-up pictures of the coal doors, rear wall of the cab and 3/4 view of the trailing truck. All pictures will be returned if so requested. If you help John, you'll help all of us when he releases his plans. By way of report, a recent "Help" item soliciting interest in On3 passenger cars got a healthy response. Let's hope the requesting manufacturer follows through.

In the News, Revell has a welcome announcement for 1/4 inch fans. The Renwal line of vintage automobiles is being produced by Revell. Available now is the 1930 Packard Victoria, 1934 Duesenberg, 1931 Cadillac and 1939 Mercedes. The model A and T Fords are being produced in Great Britain under the Revell name, but will not be imported here. However, we are attempting to discover how you might obtain the Fords. Good news for those of you hoarding those old Renwals. We'll report back.

Sadly, I must report the death of Chuck Burden, the guiding hand behind Cascade Models. I recently met Chuck and can attest that Sn3 has lost a talented craftsman and a fine gentleman. His legacy can be found on many Sn3 layouts throughout the country. He created much good will for Sn3. My condolences to his family. Cascade reports that they will complete the Sn3 plantation loco project mentioned last issue.

As for me, I must say goodbye until November. Letters are always welcome (as are SSAE's). Take care until November.

CHARLIE GETZ
845 Ridge Court
South San Francisco CA 94080

DRY TRANSFERS AND DECALS

DM



GON
HO

SSAE TO
125 LINE
29319 ELMWOOD CT.
ST. CLAIR SHORES MI. 48081

1.00



OLIVER STEAM RAIL & NAVIGATION

Custom NICKEL PLATING for
the super detailer or scratch
builder on brass or steel

HEADLIGHTS TO
SUPERSTRUCTURES

SSSAE

124 SW Yamhill Portland Ore. 97204

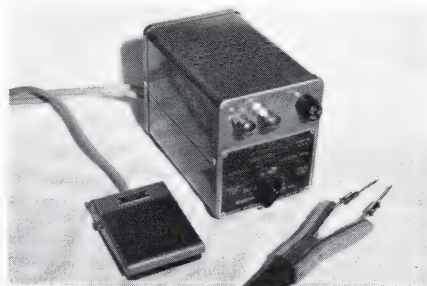
NICKEL PLATING

Think
for:



PROFESSIONAL

Soldering tools like the:



Soldering Tweezer Set

Introductory Offer*

\$84.95 ppd.

More info? Send 15¢ stamp!

P-B-L

Box 749, Chama, NM 87520
505-756-2419

*Price increase due Oct. 15th

NORTHERN NARROW GAUGE

by David G. Steer

The modern history of the Yukon begins with the gold rush of 1898. Born into this gold fever was the White Pass and Yukon railway, now the last survivor of the narrow gauge mining boom railroads in North America. The railway was built in the span of 18 months to transport men and equipment from Skagway on the Pacific coast to Whitehorse in the interior. From there the waters of the Yukon River could be used for further transportation inland to the gold fields near Dawson and the Klondike River. When the rush was over in 1901 the railway settled into a small but steady trickle of business from the permanent northern community. By diversifying into river steamer operation and by sheer determination the company survived the long northern economic depression to become the hero of the moment in 1942 during the building of the Alaska Highway. In the decades following, a very serious and farsighted effort at modernization has allowed the railway to keep pace with the recent rapid northern development and now the White Pass and Yukon transportation system is a vital part of the economy. With its narrow gauge trains climbing from the sea up the sides of the White Pass and through the rugged northern scenery, the White Pass Railway has remained a spectacular operation. The regularly scheduled mixed passenger and freight train between Whitehorse and Skagway provides an exciting and enjoyable ride for its passengers and tourists.

The White Pass Railway project began in 1898 when Sir Thomas Tanerere representing English financiers and Mr. Michael Heney, a railway engineer, met in Skagway after examining the route over the White Pass. Heney had previously met with considerable success during the construction of the mountainous sections of the Canadian Pacific Railway in 1885 and was able to convince Sir Thomas that a rail link from Skagway to Whitehorse was practical. The railway promoters purchased rights from George Brackett, the owner of a toll road on the south side of the White Pass. This trail provided the path for the first few miles of track. Further along, the rails followed the route surveyed by William Moore who had been sent by the Dominion Government to explore the area in the late 1880's. It was Moore who had named the pass he discovered after Sir Thomas White, the minister of the interior.

The railway company was organized in

April 1898 and construction began on May 27 after the arrival of materials from Seattle. The first three foot gauge train operated four miles out of Skagway on July 21 and by February 18, 1899 the track had reached the summit of the White Pass. By July 6, 1899 the track had reached Lake Bennett in British Columbia. This was just in time for a flood of gold seekers to reach the lake and raft their way to the gold fields. The first southbound train leaving from Bennett on that July day carried over \$500,000 in gold extracted from the distant river beds.

Railway construction had also started from Whitehorse and the two crews met at Carcross at the end of Lake Bennett on July 29, 1900. The last spike ceremony was held at Carcross. The distinguished participants enjoyed too much of the Yukon hospitality and the battered spike emerged the winner. The ornamental spike was removed and replaced later by the section foreman and the railway settled into the fury of transporting men to the gold scene.

Now headquartered in Vancouver B.C., the White Pass and Yukon Route provides an integrated transportation system from the south to the north. Settling on a container design long before other companies tried it, the White Pass system allows freight destined for the north to be packed in containers and loaded aboard White Pass container ships for the voyage to Skagway. At the dock in Skagway the containers are loaded onto steel flatcars for the rail trip inland. At Whitehorse the containers are transferred to trucks for delivery almost anywhere in the Yukon Territory. The result is an efficient and safe transportation system.

The major sources of bulk traffic in the southbound direction for the railway are tungsten, silver, lead and zinc mines in the Whitehorse vicinity. The largest of these mines is the lead-zinc Anvil Mine about 8 hours by truck from Whitehorse. One truck every 45 minutes leaves from this mine for the railhead. Special hopper containers are used to carry ore and concentrate. From the trucks containers are loaded on flatcars near Whitehorse for the rail trip over the pass to Skagway. At the coast the ore is dumped at a small processing plant and loaded in deep water ships for transport. This ore is destined for smelters in British Columbia, the United States, and in Japan.

During the summer months two trains regularly travel each way over the line. A mixed train leaves each end at 10:00 A.M. local time with a load of passengers and freight. The typical train consists of a half dozen container cars with ore hoppers or boxes, 8 - 10 parlor cars filled with tourists, a boxcar to handle baggage, a few flatcars with automobiles of tourists and campers, and a coach with a cupola to serve as caboose. The coaches are a mixed assortment of original White Pass stock, second hand items from the south,

and modern steel units made in Japan. Nearly all the older cars have been modernized with smooth sides and efficient oil heaters. Open platforms are still a welcome feature on all cars however.

Locomotives are typically 90 class shovel-nosed locomotives designed specially for the WP&Y by GE. Eleven 900HP locomotives of this type were purchased between 1954 and 1966. Numbers 90 and 91 are no longer in existence. Number 92 without the pilot plow is the Skagway switcher. In recent years the railway has purchased 10 DL535E diesels from MLW in Montreal. These are 6 wheeled 1200HP units of conventional design except for special air and snow fittings. The newer units are used mainly on the ore and the excursion trains.

In the evening ore-freight trains run in each direction. This train includes mainly ore containers; however, tank cars with jet fuel or heating oil, and regular box containers may also be along. On the two evenings I was in Skagway in 1975 the train left town about 9:30 P.M. with 60 hopper containers, a few mixed containers, and a steel caboose. The trains were headed by 5 MLW locomotives.

During the tourist season an excursion train is operated on days that cruise ships dock in Skagway. This train departs at 8:30 A.M. from the boatside and travels the most scenic part of the line as far as Bennett. The train returns about 4 P.M. This gives the passengers a few hours to look around town before the boat leaves in the evening.

The most difficult part of visiting the White Pass Route is getting there. The WP&Y is in the far northwest of Canada on the Alaskan border, a long, long way for most of us. Probably the easiest and quickest way to get there is to fly. Scheduled air service is available to both Skagway and Whitehorse. I flew to Juneau and took the South East Skyways air service to Skagway. This is about a one hour flight and is almost worth the trip north in itself. The small amphibian aeroplane travels up the Lynn Canal between the mountains (not over them) and affords a magnificent view of the rocks and glaciers. The plane lands at Skagway after circling the valley to give everyone a view of the town and its surroundings. On the return journey I flew south from Whitehorse in a conventional jet. Canadian Pacific Airlines operates daily service to Whitehorse from Edmonton and Vancouver.

The most leisurely way to reach the White Pass would be via one of the many cruise ships or ferries that ply the northern waters. The ferries leave from Vancouver or Prince Rupert and sail to Skagway. The trip provides a relaxed view of the inland waterway of the Pacific coast. The Canadian Pacific operates the Princess Patricia in this service each summer. The Alaska ferry system also operates ships from the south to points in Alaska. These ferries carry cars and provide Skagway with its



Skagway Harbor. The building on the left is the ore processing plant and dock for the ore ships. In the center is the dock for the Alaska Ferry System car ferry ships. At the right is a cruise ship docked at the White Pass wharf. The town of Skagway is in the valley hidden by the ship. *Photo by David Steer.*



Containers of the box and hopper type are shown in this photo. These unique narrow gauge cars carry much of the freight on the WP&Y. *Photo by David Steer.*



This triple hopper once ran on the East Broad Top Railroad. It is used as a ballast car. Since the WP&Y does not interchange with any other railroad there is no need for elaborate lettering schemes on its rolling stock. *Photo by David Steer.*



This diesel, photographed outside the Durango, Colorado roundhouse on April 16, 1960, turned up 15 years later in service on the WP&Y as their #81. *Photo by Robert Grandt.*

only road link to the outside. A road is under construction north from Skagway to connect with the Alaska highway near Carcross, however progress in the northern mountains is slow.

The most ambitious way to reach the White Pass is to drive the Alaska highway to Whitehorse. Probably this is the most economical way; however, it is not a journey to be undertaken lightly. It is 350 miles from Edmonton to the beginning of the Alaskan highway at Dawson Creek and another 1000 miles of unpaved road to Whitehorse. It is best to travel in June to take advantage of the long northern evenings. Many people do drive the route and put their cars on the train for the trip to Skagway, and then proceed further north or back south via the Alaskan ferry ships.

Although the casual tourist can see all of Skagway in a few hours, the serious narrow gauge enthusiast should plan on spending between one-half and a full day as there are a number of not-so-tourist things for the railfan to see. There are two locomotives on display; #52 (formerly #2); an 1881 Brooks mogul sits across the street from the station and #195, an ex-U.S. Army engine, sits beside the city hall and the Trail of '98 Museum. The museum is small, but interesting, as it has a number of photos of early White Pass operations including shots of the unusual outside frame 4-6-0's. At the north end of town the railroad shops and yard can be seen. Various items of White Pass stock rest here between assignments. Once containing a 20 stall roundhouse, car and loco shops, and a coaling trestle, the area is less crowded now. The roundhouse was destroyed by fire in 1969 and has since been replaced by a rectangular functional steel locomotive repair shop. Several wooden car and locomotive sheds still remain in use and the area contains numerous relics of the past including several old tenders from army locos, hulks of diesels from the roundhouse fire and even the remains of the turntable bridge. The area is surrounded by a large balloon loop which is used to turn the passenger trains at the end of their trips.

Walking back through town modelers will note the many old box and refrigerator cars now being used as storage sheds in many backyards. These cars are a gold mine of information about White Pass rolling stock before the containers took over. A stop at Dedman's Photo Shop on Broad Street is worthwhile as they have an extensive collection of slides and black and white prints of the railway for sale.

The journey on the train for the 110 mile trip from Skagway to Whitehorse begins around 10:00 A.M. One should wear a sweater or jacket for the trip as it gets cool in the higher levels of the pass. The station is located at the foot of Broad street at the harbor. The track from the station has been relocated from its original route down the middle of Broad street



A typical WP&Y container on one of the newer roller bearing flat cars. The flat car is black with white lettering while the container is aluminum with green letters. The container number also appears in large 24-inch numbers across the top of the container. *Photo by Robert Grandt.*



These depressed center flat cars are unique to the WP&Y. They are home made 40-foot cars. *Photo by Robert Grandt.*



Formerly of the Colorado & Southern, this baggage car is colorful indeed. Painted to match the passenger equipment it is in Pullman green with yellow lettering and has a boxcar red roof. *Photo by Daryl Ryder.*

and now it skirts the mountains on the east side of town. Leaving Skagway at sea level, the railway climbs to the 2885 foot summit of the White Pass by looping and winding along the sides of the mountains. From the train windows portions of the original foot trail of 1898 can be seen. The summit is reached by means of a number of wooden trestles and a tunnel as the track clings precariously to the sides of the rocks. Near the summit a new steel girder bridge and tunnel carry the track over Dead Horse Gulch, the scene of much suffering and loss of life by the pack horses during 1898 and 1899. The original steel cantilever bridge and trestle can be seen from the train.

In the valley near Skagway the vegetation is quite abundant, but the track quickly passes the tree line and at the summit the land is very barren. Only lichen and a few tiny bushes survive among the rocks and pools of clear water. It is comforting to be snug inside a warm coach in this otherwise desolate spot. Just before reaching the summit it is possible to look back down the valley and see Skagway in the distance. While it is only a few miles by air it is 21 miles by rail. Although the south ascent to the summit is the most rugged portion of the line, it is not the steepest. The average slope here is 2.6 percent. The track on the north slope near Bennett has the steepest grade of 3.9 percent.

After the summit the track passes from Alaska into British Columbia and proceeds to Bennett at the head of Lake Bennett. Once a sprawling boomtown where the gold seekers left the railway for rafts, Bennett now contains only a substantial railway station, a Wye for turning locomotives, an abandoned church, and locomotive #73 with caboose and rotary plow on display.

Bennett is the division point of the railroad. Trains coming north from Skagway typically have five locos, however only two are required for the remainder of the trip to Whitehorse. Three are cut off and turned at Bennett, and proceed back to Skagway with the southbound train. Bennett is reached just after noon by both the north and southbound mixed trains, and the railway serves a complimentary lunch for the passengers in the station. Although the crowds are large, there is plenty of good food for all. After lunch there is just enough time for a quick look around before the trains depart.

Leaving Bennett, the track follows the shore of the lake all the way to Carcross which is the first significant Canadian settlement the railway passes, and the first road connection to the Alaska Highway and the rest of the world. The track crosses the mouth of the river on an unusual wooden swing bridge which in the past was opened for sternwheeled steamers. One such boat is on display at Carcross.

From Carcross the track wanders its way towards Whitehorse. The terrain

changes from rocky hills to sand and clay hillocks and the track winds in a serpentine fashion on its journey. The truck-train container transshipment depot is a few miles outside Whitehorse on the highway. Near Whitehorse the track begins to follow the Yukon River into town. The Whitehorse station is located adjacent to the Yukon river near the center of town. At one time the White Pass river steamers docked here and received passengers and freight for travel on the river system. Now the docks are gone and only the station and a few timbers remain.

The White Pass has another balloon loop for turning the trains in Whitehorse. There is also a small freight yard and a shop for repair work on the trains and the trucking fleet. Across the street from the station is a museum with several relic locomotives slowly decaying. Further into town, two of the largest river steamers are on display.

Whitehorse is the capital of the Yukon Territory and as the seat of government has all the facilities of modern cities including motels, television, radio, telephones and paved roads. Its road access

to the south and modern jet airport make it seem only a short distance from the rest of the world.



WHITE PASS & YUKON ROUTE

Bibliography:

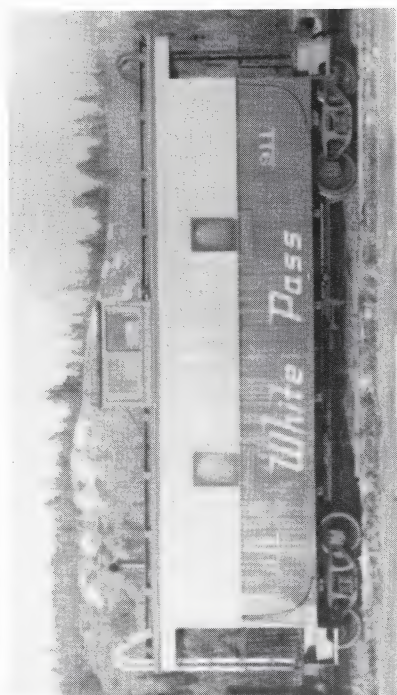
- Armstrong, John (1953). Modeling the WP&Y. *The Model Railroader Magazine*. March, p. 44.
- Cavalier, Julian (1975). Passenger Cars of the WP&Y. *Railroad Model Craftsman*. April, p. 40.
- Clifford, Howard (1975). *The Skagway Story*. Alaska Northwest Publishing Co., Anchorage, Alaska.
- Hilton, George W. (1971). Integration in the North. *Trains Magazine*. July, p. 37.
- Jaques, F.L. (1951). Gateway to the Yukon. *Trains Magazine*. Jan., p. 36.
- Lavallee, Omer (1972). *Narrow Gauge Railways of Canada*. Railfare Enterprises Ltd., Montreal, Canada.
- Martin, Cy (1969). *Gold Rush Narrow Gauge*. Trans-Anglo Books, Los Angeles, Calif.

W.P.&Y. CABOOSE No. 909

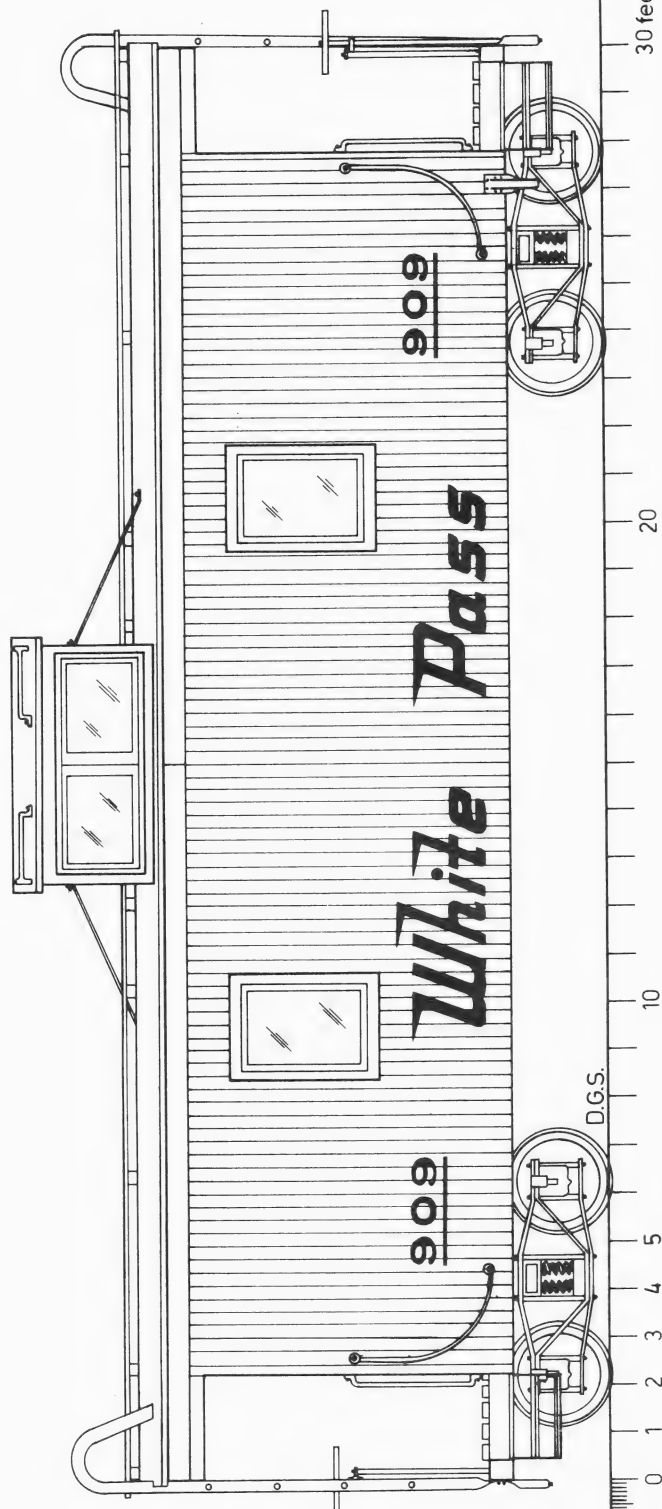
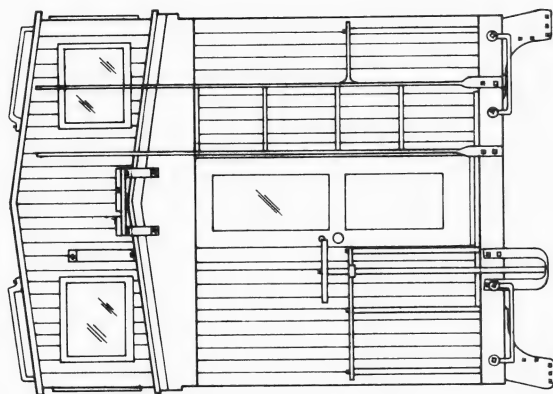
BENNETT 8-31-75

BOXCAR RED - WHITE LETTERING

Drawn by David Steer



One of the last wooden cabooses to see service was #911. This caboose had green and yellow sides and ends with a green cupola. The roof, end platform, and trucks were boxcar red. *Photo by Daryl Ryder.*

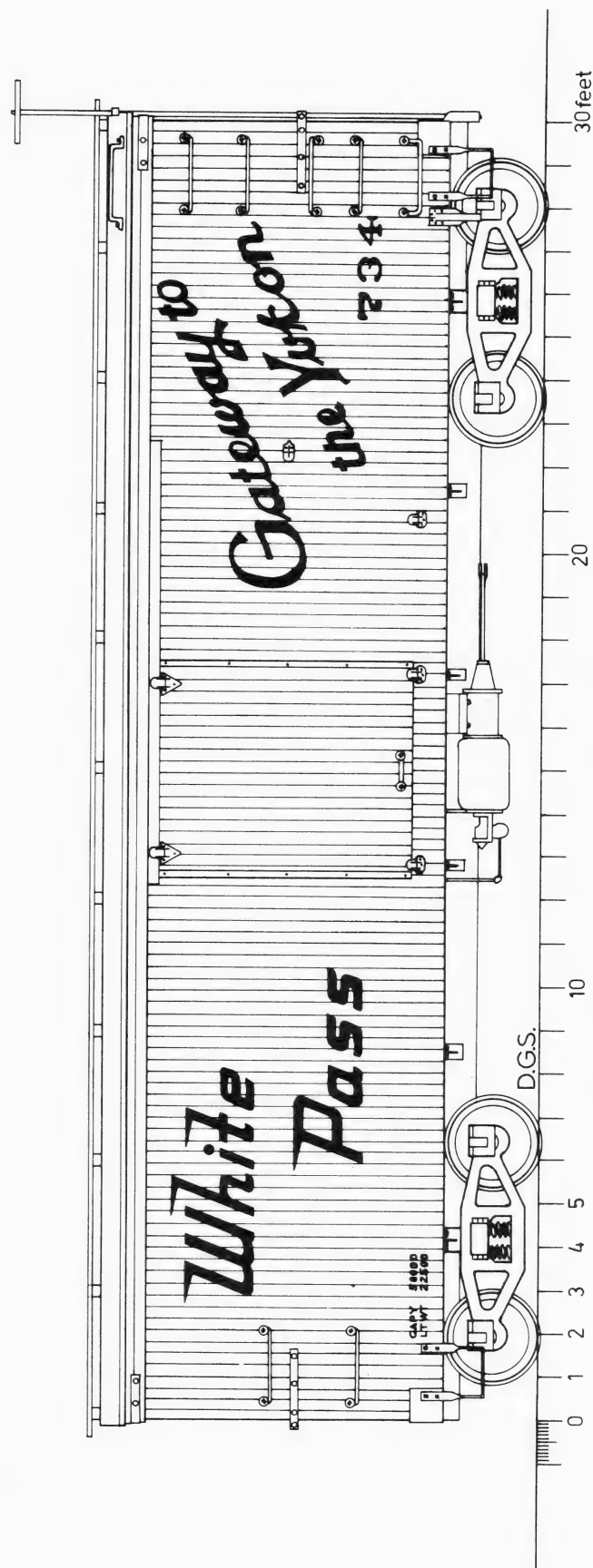
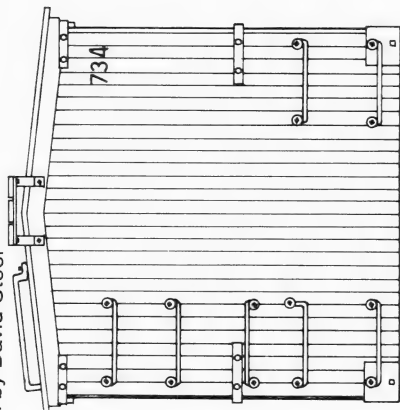
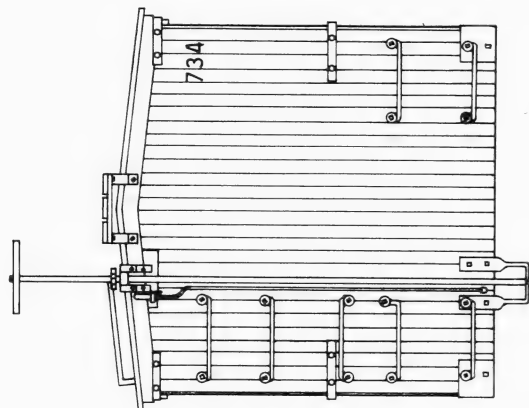


W.P.&Y. 30FT. BOXCAR No.734

SKAGWAY 8-30-75

BOXCAR RED - YELLOW LETTERING

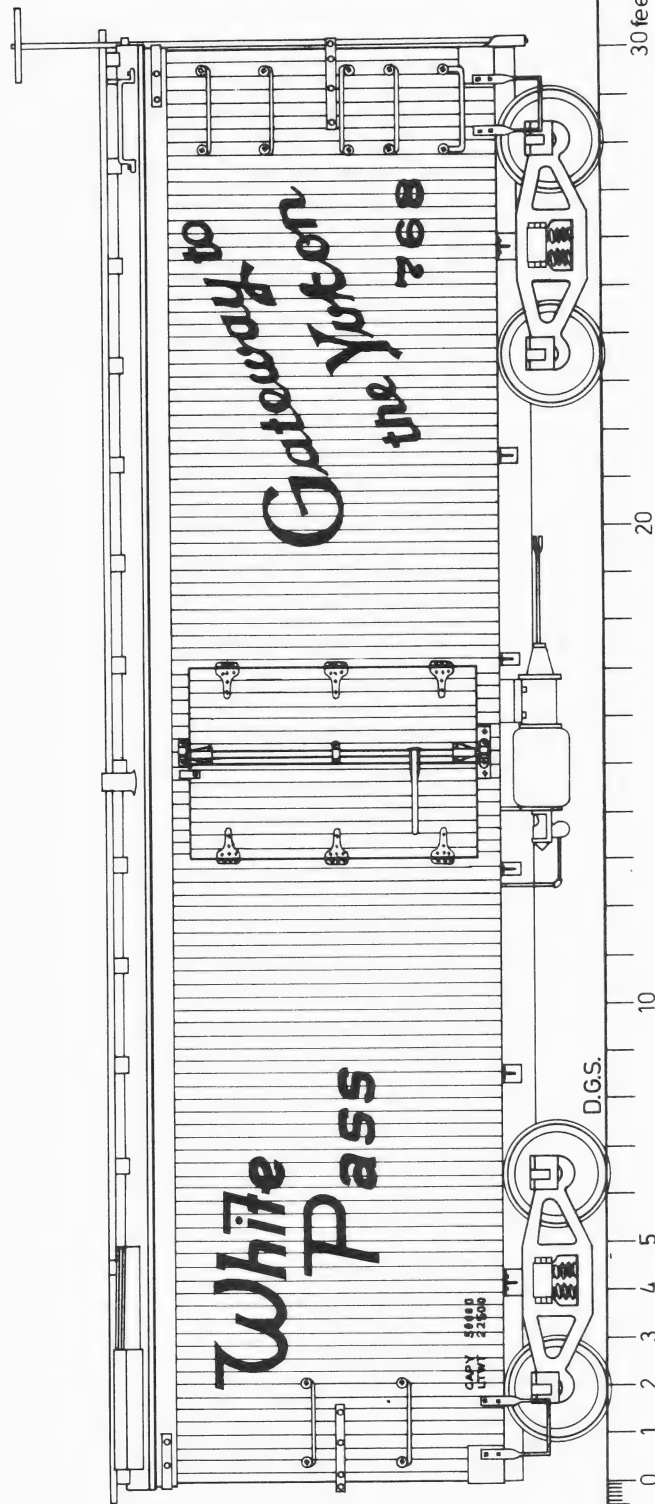
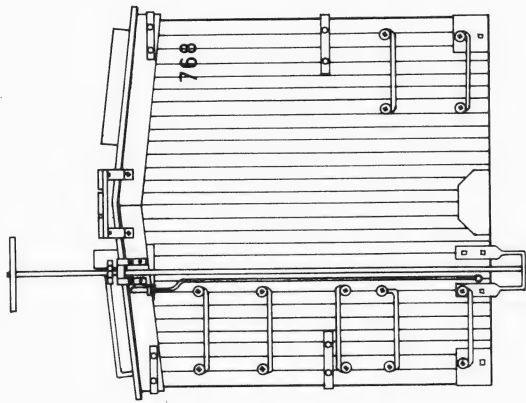
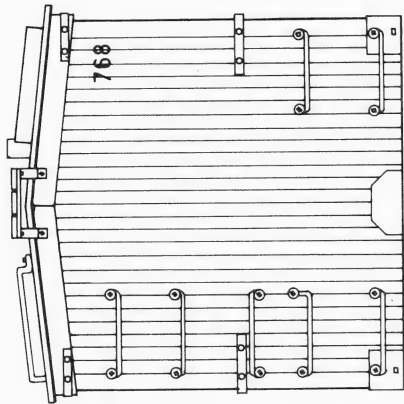
Drawn by David Steer



WP&Y. REFRIGERATOR CAR No.768
 SKAGWAY 8-30-75
 SIDES-YELLOW ENDS-BOXCAR RED

Drawn by David Steer

SIDE LETTERING - BLACK
 END LETTERING - YELLOW

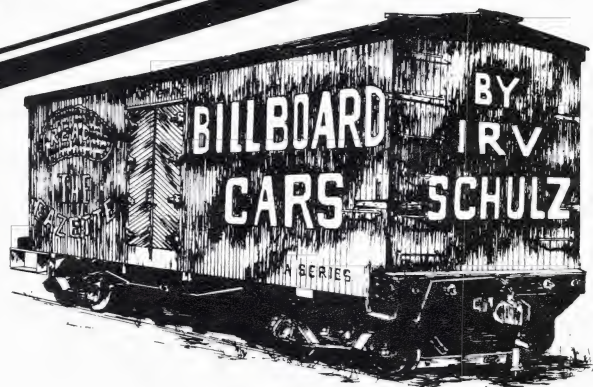




PICKLES AND VINEGAR ON THE HI-IRON

by Irv Schulz

A trainload of Heinz products leaving the yard on the St. Clair Northern Line for parts unknown. Note the two "coffin" cars right behind the locomotive. Photo by Leo Knight.



The H.J. Heinz private cars were the most colorful to roll on U.S. rails. In the December 1973 **NMRA BULLETIN** I presented some information on these cars including photos and cardstock sides of a Heinz baked bean car.

Heinz founded an empire on horse-radish, pickles, and other condiments. There is a list of references at the end of this article for those of you who want to find out more about the fascinating history of the Heinz Company. Here I will discuss some of the transportation problems involved and the cars used to solve them. The Heinz Company built and maintained many of their own cars.

The Heinz processing center was in Pittsburgh but cucumbers for pickles grew in Indiana, cabbage and cauliflower in other places. Produce was brought to Pittsburgh, processed, canned, bottled and packed and re-shipped throughout the country.

Many of the Heinz cars were not refrigerator cars at all, but boxcars; they had colorfully painted sides which lacked hatches and bunkers, and contained case after case of nonperishable items. The odd fact is that all of these cars were equipped with refrigerator doors and hinges!

"57 Varieties" was done in at least three different ways with variations to both the "57" and the V and S of the word varieties. Actually each and every car had its own unique lettering and was painted strictly by the whim of the person handling the car stencils.

If this doesn't throw one seeking some kind of uniformity, the following will. Each car had a different shaped pickle painted on its side. The color of the pickle was always green, with the word "Heinz" in white centered in the pickle. Sometimes the word "Heinz" was shaded in black or the pickle shaded in white, de-

pending on the person handling the car stencil.

A 1908 railway equipment list shows the Heinz cars were numbered as follows: Refrigerator even numbers 400 to 502, Vinegar tank cars 50, 61, 62 and 63. Pickle tank cars 18 to 20 with miscellaneous numbers up 67 and 68. There were also refrigerator cars from 300 to 394; certain of these cars had reporting marks for "Armour Refrigerator Lines" in Chicago.

Early Heinz cars were painted with white sides, green roofs and ends, and black underframes. The lettering as shown in Figure 1 was black, red and gold. The "coffin" cars used to haul pickles and vinegar were also white with the same roof and end schemes. The senior Heinz is reported to have helped design these "coffin" cars and was the first to ship pickles or vinegar in cars of this type.

Somewhere around 1900 the cars were

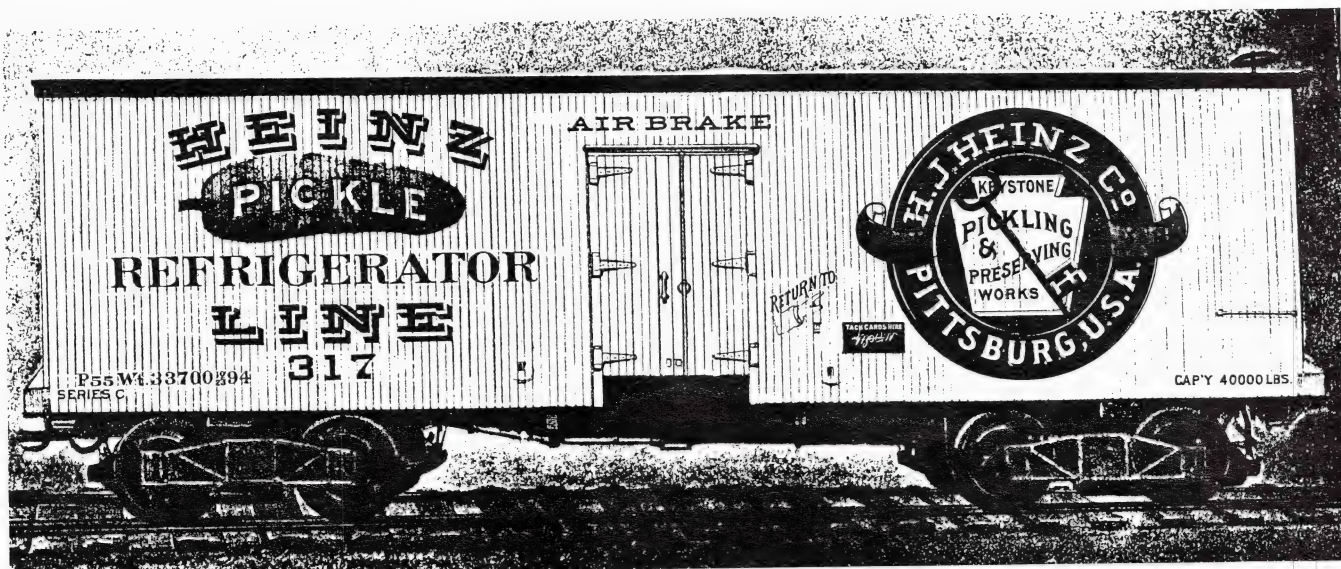


Fig. 1 — An early Heinz refrigerator car taken from an 1894 salesman's catalog. The catalog notes that Heinz inaugurated his own refrigerator line in order to have goods reach the customer in every climate and in the best possible condition. *Courtesy H.J. Heinz Co. Collection of Ed. Lehw.*

Fig. 2 — The main Heinz plant which still stands today. *Photo, courtesy Carnegie Library, Pittsburg. Collection of Irv Schulz.*



repainted featuring yellow sides, red lettering with a white background and the traditional green pickle.

Figure 2, which shows one of these cars, is a rare photo which I recently acquired. It tends to take you back to the age and era where it all began. The sign in itself would be a nifty project for the modeler.

Figure 3, which shows two of the "57" cars, was probably taken in the early 1920's.

It is not difficult to build cars like these, especially in HO. I use and recommend LaBelle kits coupled with Grandt

line fittings for most of these cars. LaBelle kit HO-48 is a natural for the boxcars and kit HO-41 can be cut down to duplicate the "coffin" cars. The sides and sign made of styrene are natural here. Rail fittings are nothing more than boat stanchions, and are available at most better stocked hobby shops. Lettering for several of these cars is available in HO only from the 125 Line.

Even though Heinz had little to do with short line railroading, I feel that the photos presented here will preserve a little of our vanishing Americana.

References:

Alberts, R.C. (1973). **The Good Provider**. Houghton Mifflin.

Alberts, R.C. (1972). **The Good Provider**. *American Heritage*, p. 23, #2 (February).

Schulz, Irv. (1973). **Bulletin Car-Heinz Baked Beans**. *NMRA Bulletin*, Vol. 39, #4, (December).

Fig. 3 — These later cars were repainted and renumbered sometime around the early 1920's. Note the absence of ice hatches. *Photo, courtesy Carnegie Library, Pittsburgh. Collection of Irv Schulz.*



NARROW GAUGER'S Parts - Kits - Decals

- Operating HO_{n3} layout •
- Can Motors/Flywheels •

Come pay us a visit!

Tu-Fr 10-9
Sat 10-6, Sun 1-5



THE TRAIN EXCHANGE
70 Hilliard St.
Manchester, Conn. 06040
(203) 646-0610



The Toy & Hobby House Scale Railroad Division

WE STOCK IT ALL, O to N Scale
Standard & Narrow Gauge
Over 250 Lines!

R-T-R • Kits • Built Ups • Books
Parts & Pieces • Custom Painting

YOUR KIND OF HOBBY SHOP

Stop In, Call or Write Us ...
We're Open 7 Days A Week
We Accept Master Charge

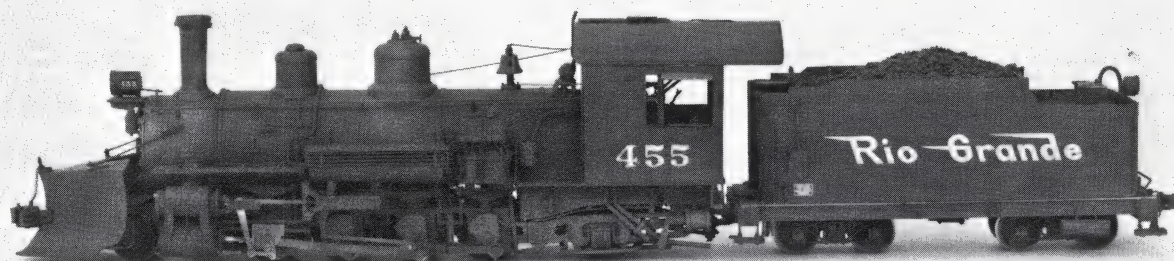
The Toy & Hobby House
Scale Railroad Division
Phone: (312) 827-6550
1486 Miner Street, Des Plaines, Illinois 60016



PRODUCTS

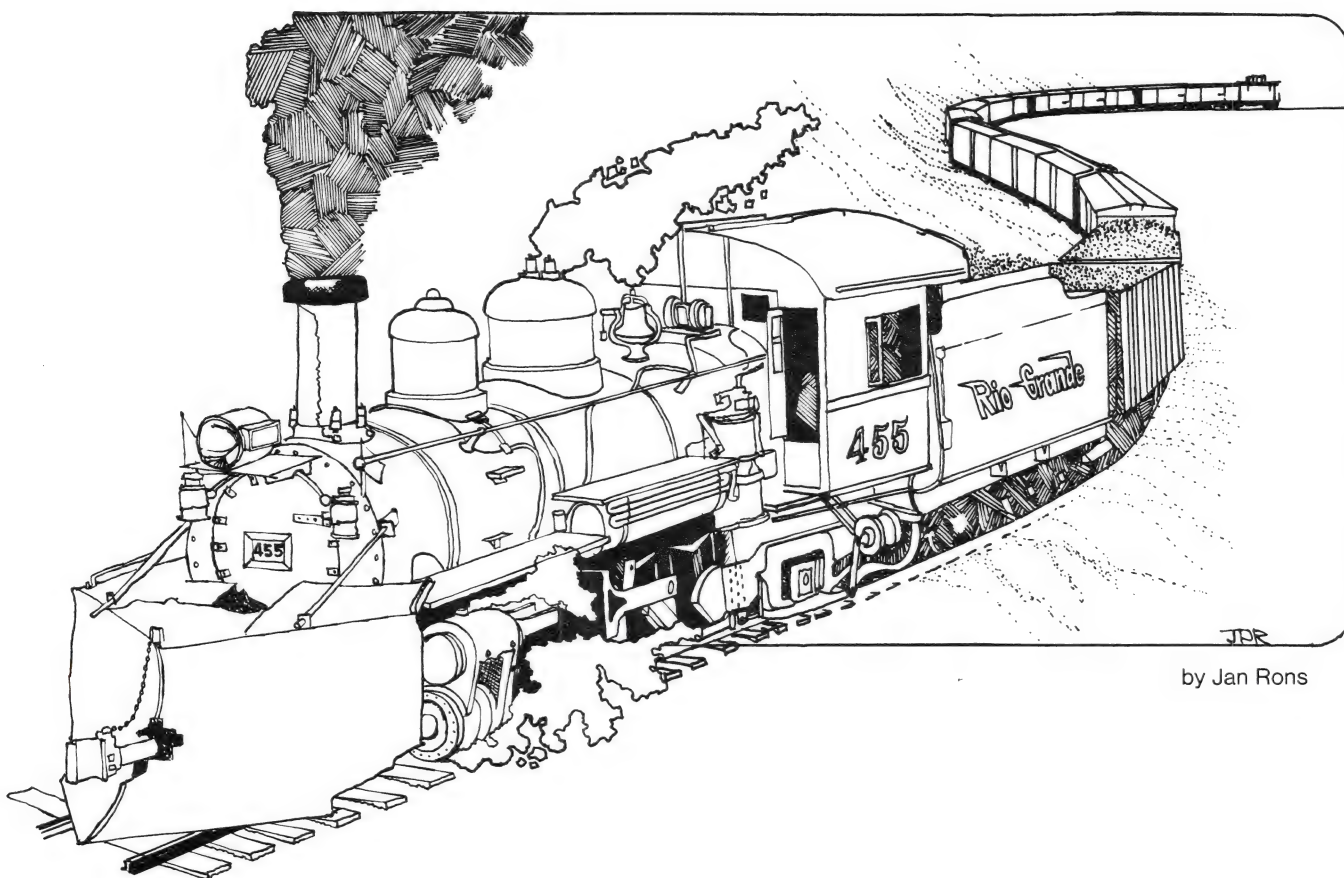
Light weight rail and scale weathered ties for the narrow gauge and short line modeler in HO or O scale. See your dealer or send SSAE for list of all track products, Micro Motors and gearheads.

2201 Atwater St. Louis, Mo. 63133



Rio Grande Southern
#455 in Sn3

by Paul Washburn
Photos by Richard Christ



by Jan Rons

“Best in Show, PCR-NMRA — Phoenix, Arizona, 1978”

Ever since I started to model in the rare scale of Sn3, I had toyed with the idea of building a K-27. Finally I did it and this article describes how. Being basically a freelance modeler, I had built many Sn3 locomotives by using HO mechanisms, reworking them, adding new superstructures, and otherwise changing them until I was perfectly satisfied with the results. To build this K-27, I scratchbuilt a model of a specific prototype using research, plans, and photographs.

Deciding which of the fifteen K-27's to build was the easy part. It *had* to be Rio Grande Southern #455 as it appeared in June, 1951 when it had the steel cab and the tender from #452. This locomotive went into service on the RGS in 1939 after the RGS traded a steam ditcher to the Denver & Rio Grande Western for it. The time after this was the time in its fifty year history that interested me most. It was the first RGS locomotive to have the new herald painted on its tender. In 1947 it was rebuilt after being the victim of a bad wreck in 1943. Some of the rebuild parts came from standard gauge locomotives. The steel cab came from a Denver & Rio Grande Western standard gauge Class S-33 0-6-0. The tender tank came from a D&RGW standard gauge 2-8-0 making this a most unique looking K-27. Somewhere along the line in 1951 #455 acquired

the tender from #452 but the tender number was not changed to match the locomotive until some time later. After half century of service #455 was scrapped in 1953.

At first there were only a few parts available for the K-27 in Sn3. Like most Sn3 modelers I was waiting for the main construction parts such as the frame, cylinders, drivers, etc. In late 1977 the drivers and counterweight assemblies became available from Tomalco. After obtaining a set, I decided not to wait any longer and gathered up all the parts I had accumulated for the project and went to work.

I started by modifying Allan Brewster's plans for K-27's from the **MODEL RAILROADER**. The cab, cylinders and tender were changed to conform with the prototype. I also added the steel snow plow. The steel cab was a guessing game. I had no dimensions available to me so numerous photos were used as guidelines. The cylinders were a minor modeling problem in that they had been rebuilt and the jackets were never replaced on the prototype. I was fortunate to obtain some photos from Roger Schenck that he had taken of engine #464's cylinders when it was being rebuilt at Knott's Berry Farm. A John Maxwell plan on snow plows was used for the basic plow. Photos were used

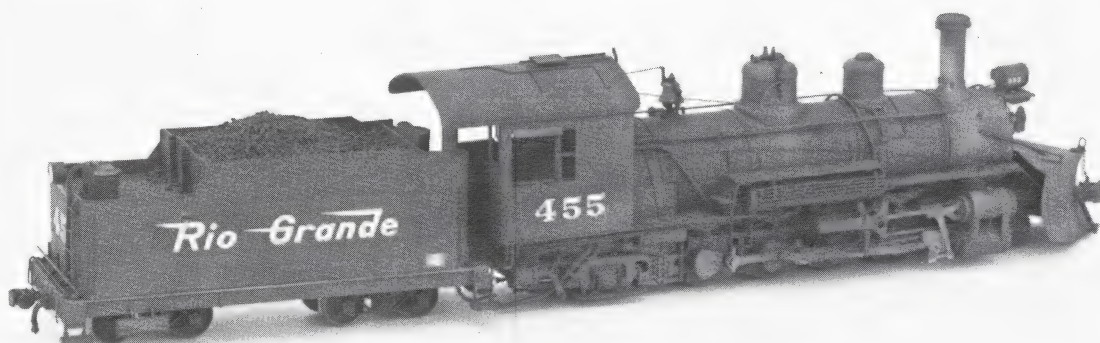
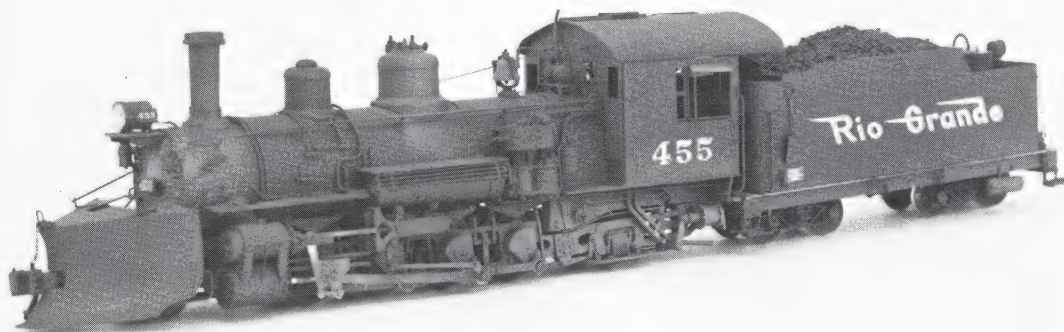
for details such as the very large patch job on the right side of the plow on #455. The cab, cylinders and snow plow play a very distinctive part in the appearance of this locomotive. The remainder is basic K-27 for that time period.

With the exception of the two tender frame end beams that are made of wood, the model was constructed from brass sheet stock, brass tubing and various commercial parts and castings. Scratch-built parts were cut out by hand using a jewelers saw and a variety of small hand tools. Most drilling was done on a small drill press and turnings were made on a Unimat. All brass parts were soldered together using a small torch, soldering gun and soldering tweezers.

The model is made up of about 600 individual pieces. It is powered by a #250 micro-motor, 3.45 gearhead and a 28 to 1 KTM gearbox. It was painted and weathered by airbrush and Thinfilm decals were used.

In the six months time period it took me to complete this engine, I feel that I have increased my modeling ability and raised my standards of modeling. I can honestly say that I enjoyed every minute of it.

(Allan Brewster's K-27 plans were published in the March 1973 MR.)



Colorado Narrow Gauge Tank Cars

Part 2: The TEXACO Cars

by Robert E. Sloan

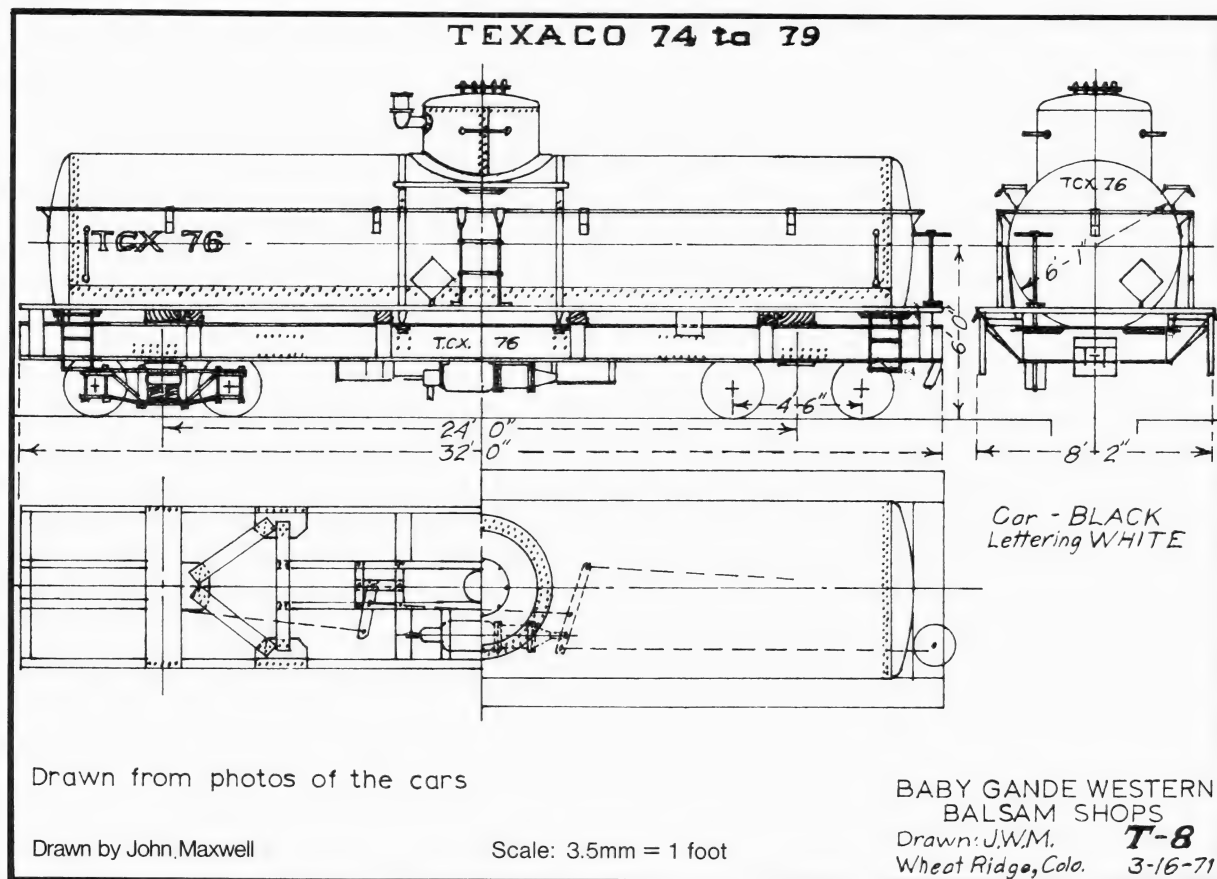
In part one of this three part series I discussed the UTLX narrow frame and frameless tank cars of the Colorado narrow gauge railroads. In part 3 the CONOCO-CONX narrow gauge tank cars will be described.

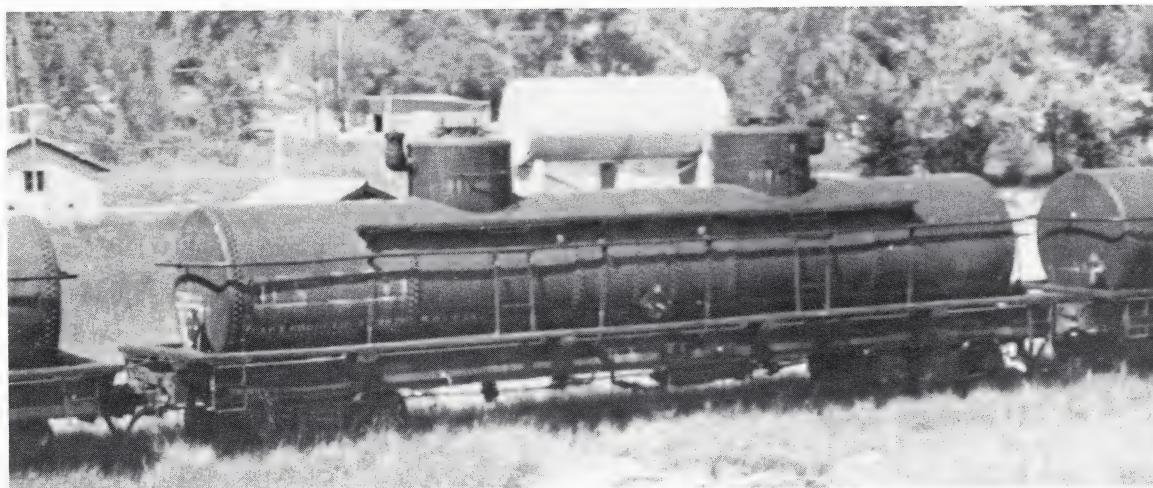
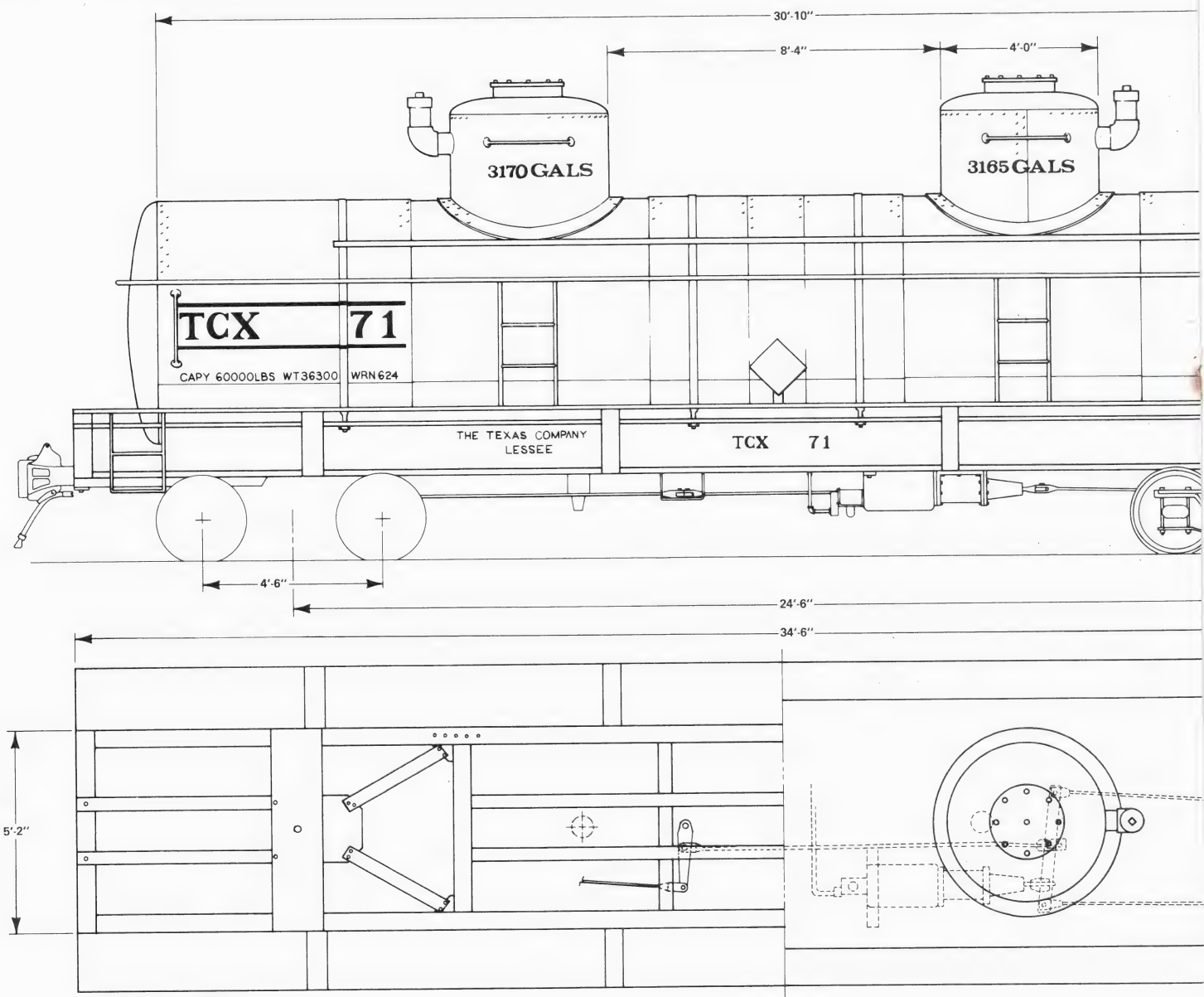
The Texas Co., or TEXACO, leased 10 narrow gauge tank cars which were number CYCX 60 to 69 from the Pennsylvania Conley Tank Car Co. of Pennsylvania. This company was a subsidiary of the Texas Co. which was absorbed by GATX in 1943. They were 60,000 pound cars exactly similar to the UTLX narrow frame single dome tanks cars described in part one of this series. The CYCX narrow gauge cars were gone by 1943. Later

TEXACO leased 10 more narrow gauge tank cars from GATX. These were numbered TCX 70 to 79. 70, 71, 72, and 73 were two dome tank cars with 60,000 pound and 6335 gallon capacity. 74 to 79 were 60,000 pound, 6000 gallon single dome cars. All of the TEX cars were mounted on the same 4 foot 6 inch wheelbase arch bar trucks used on most of the CONOCO cars. The TCX cars were all listed in the Official Guide in 1945 but were gone by 1955. John Maxwell mentions that they were shipped away from the Alamosa-Antonito area in July 1950. The numbers formerly assigned to these tank cars were reassigned to much larger standard gauge tank cars.

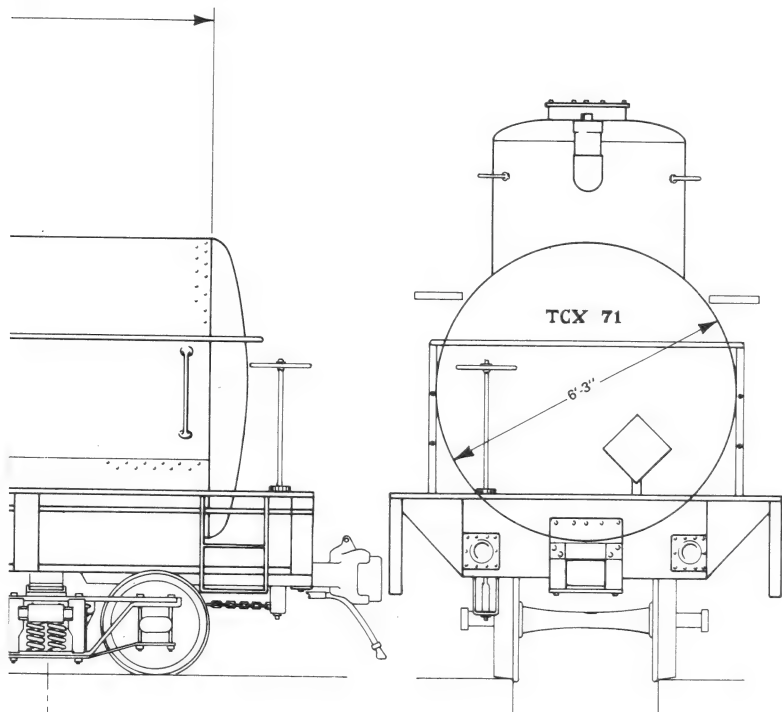
One last note on numbering. John Maxwell also mentioned that his records show that the TCX tank cars 70 to 79 apparently did not appear on the D&RGW-RGS narrow gauge until about 1935 since they are not listed on the equipment registers until 1937-38.

No one that we know of managed to measure the two-dome tank cars. However, several photos exist. By taking wheelbase dimensions and gallonage it is possible to re-construct measurements from the photos. Maxwell has drawn #70, Pruitt drew #72 and I have drawn #71. All of these plans were done independently and the measurements are similar in all three.



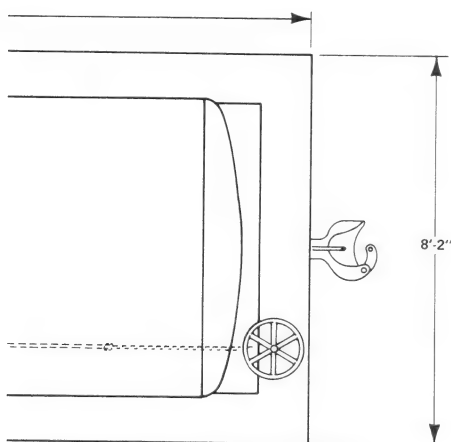


TCX #71 photographed at
Photo from the Robert E.

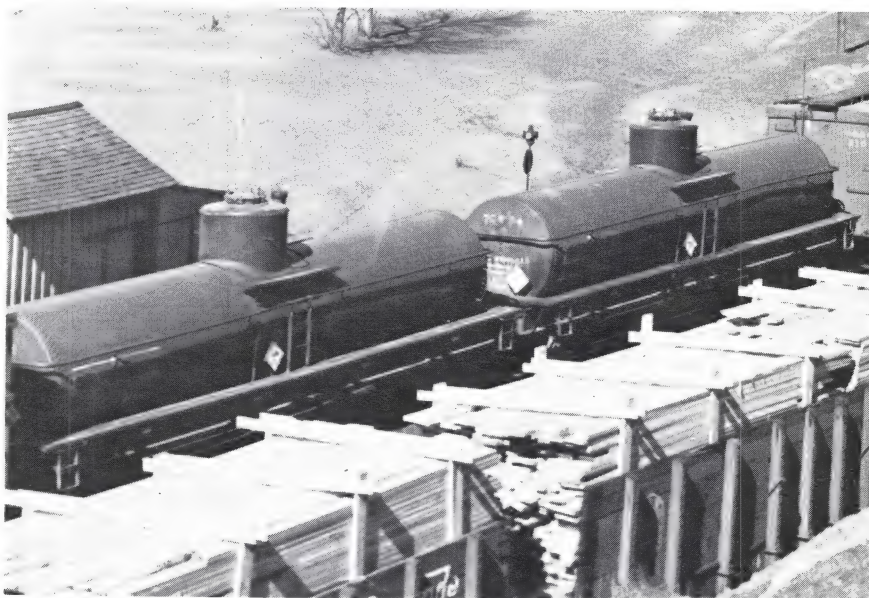


TEXACO TWO DOME TANK CAR #71

Drawn by Robert Sloan
Traced by Dick Whitney



Scale: 1/4 inch = 1 foot



TCX single dome narrow gauge tanks cars #76 and #74 being switched in the Durango, Colorado yard on March 1, 1946. Photo by John W. Maxwell.

Finescale ON3 Rebuilding Techniques

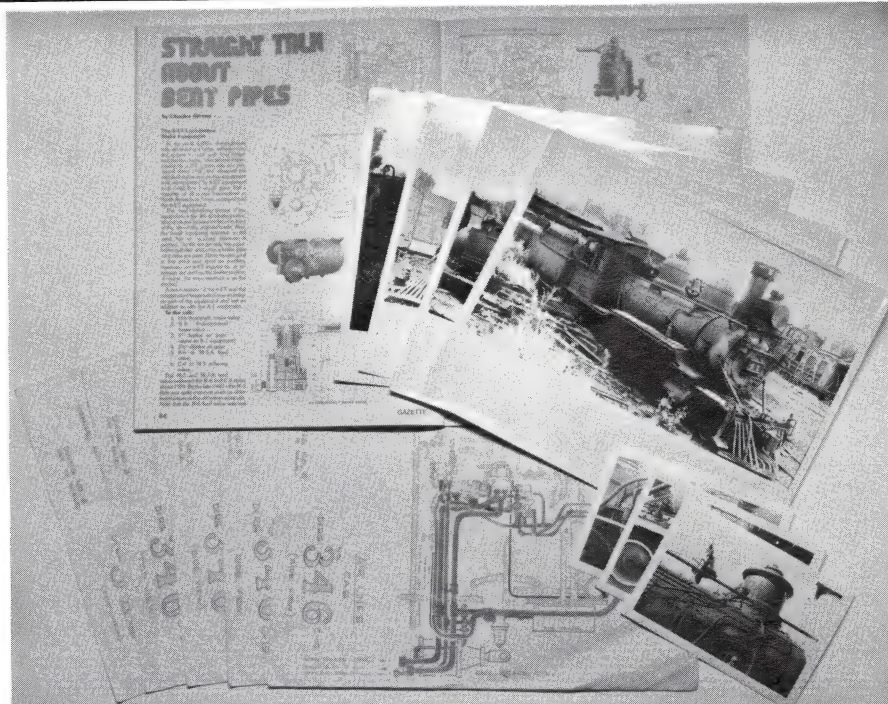
Part 1: The Balboa C-19 Becomes RGS #41

by Kelley Morris
Photos by the author

Although the quality of brass imports in the last few years has improved drastically, many individuals still own earlier engines which cannot come close to the current quality and detail level of today's top models. This series of articles is aimed at those people who wish to "update" their older imports to contemporary standards. It will be an occasional series in which I present techniques suitable for rebuilding brass locomotives in any scale or gauge.

The Balboa C-19 in ON3 is a fine running engine. It requires little in the way of running gear modification, with the exception of a micromotor, yet it lacks

much in terms of the sophisticated detail we have come to enjoy. The following rebuild concept is within the scope of the average modeler if he has, first, worked with metal before and/or second, if he is not intimidated by completely dismantling an engine, discarding one third of the parts and starting over. It is harder in this case to attempt to attach parts without disturbing existing ones than to dismantle completely. In this article I show how I disassembled the Balboa C-19 and started to put it back together. Remember, these techniques can be used in rebuilding any engine in any scale.



Samples of reference data including magazine articles, photos and field notes.

STEP 1: RESEARCH

Since, within reason, we want the model to be as accurate as possible, we need to know as much about the subject as possible before the rebuild begins. I have taken thousands of detail photographs over the past ten years, and done sketches of the plumbing systems of numerous intact locomotives. Thus I need to do little bush-beating to get the information I need. I use as many different views as possible. If I don't have enough I gather data from books, magazines, or sources such as John Maxwell, Harold Vollrath, Gerald Best, Richard Kindig, etc. Bob Richardson, at the Colorado Railroad Museum, has been of help to me on numerous occasions. I review the overall scheme and uncover particular idiosyncracies of

the subject.

C-19 #346 resides at the Colorado RR Museum. RGW #41 itself (somewhat modified from its original configuration), as well as D&RG #340 (also a C-19), are alive and well at Knott's Berry Farm in California. If you can't make a trip, photo sets of #346 and others, which cover details, including cab, of the entire locomotive can be purchased. In addition, you might refer to the series of articles published in the **GAZETTE** - "Straight Talk About Bent Pipes" which covers the basic functions of steam locomotive piping in layman's terms, or the various reprints of the locomotive encyclopedias. In general, *know your engine first, before you start rebuilding.*



The soldering tools used in this series. The acetylene prestolite torch is on the left, a striker for lighting the torch is shown in the center, and a small soldering iron is shown on the right.

STEP 2: DISASSEMBLY

I use an acetylene prestolite-type torch for everything save the wire work. While many modelers have successfully used the various resistance soldering machines, I still find the torch quite fast and acceptable. You make your own choice here, but this series will be oriented to acetylene and air torch usage. Those wishing to purchase this type of torch will find them at their local welding or jewelry supply. Allcraft Tool and Supply Co., California Lapidary Supply Co., and many others will mail order sets including regulator, hose, tips, etc. Acetylene tanks must be obtained locally. Expect to pay \$50-\$60 for a good system plus tank. Note that most of the solder used in imports flows at about 400-450° F., and although a propane torch will easily heat locomotive structures to this temperature, they are awkward and have limited flame (tip) size. Also note that the prestolite type torch requires only acetylene (it draws its own air through the tip), and is reasonably economical to operate once purchased. A "B" size cylinder usually costs about \$7.00 to refill and normally supplies enough gas for an entire rebuild. Compare this system to a micro-flame or similar system and the long range savings will be apparent.

Disassembly should be a careful process, sometimes taking as much as an hour. My normal procedure is to undo all mechanically held (i.e. screwed) parts first, then strip the loco in lacquer thinner. On the C-19, this includes: superstructure, frame, cylinder assembly, motor mount, stack, domes, number plate, tender shell and trucks, motor pickup and drawbar, etc. Before removing the drivers it would be wise to scribe lightly around them (against the frame) as this will help to indicate frame cutouts and brake hanger locations later. Using the #2 tip supplied with your torch adjust the flame to a medium intensity, and begin heating the general area of the part to be removed. For example, to remove the superstructure: Hold a good set of strong and *long* (heat

travels) tweezers in one hand and the torch in the other. Once the melting point of the solder holding the part has been reached, a gentle pressure should pull the superstructure free. Start at the front of the boiler and work toward the cab, occasionally letting the structure cool to avoid things "falling off" too quickly. In this case, we're removing everything from the boiler but the pump brackets, washout plugs, backhead and the generator cab floor and pedestals. Care must be exercised around the cab if the soldered on detail on the cab sides, i.e. windows, arm rests, and so on are not to be removed. A good way to avoid unwanted dislodging of parts is to focus the heat on only the immediate seam in question rather than the overall surface. Experience will be a great teacher here, as the lessons learned in disassembly will directly apply to reassembly. In removing parts, heat applied to one part will generally suffice, but in "sticking things back together", the heat must be even in both parts being soldered to effect a strong, clean joint. Wet paper towels or tissue wrapped around areas not to be unsoldered may also be helpful. In addition to unsoldering all the parts, the firebox door is ground off the backhead casting (unless a very early vintage model is desired). Experienced modelers might wish to unsolder the backhead and mill away this

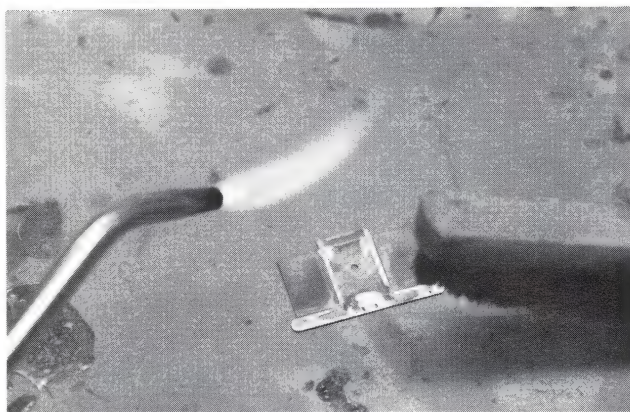
detail, but due to the bevel on the backhead, repositioning it exactly right in the boiler can be difficult. Also at this time file away the deck brace and builder's plate etched detail on the smokebox. The particular engine we're duplicating is R.G.S. #41, and the prototype did not have a steel cab. The model's cab is of sister engine #346 which was wrecked on the C&S in the 1930's; the cab was replaced with a riveted and welded steel unit. Therefore we must file or unsolder all rivet detail including the cab roof and extension. Those not wishing interior cab detail on their model may leave the cab in place to remove the detail. Be sure not to dislodge thickeners and weight mounts in the boiler.

Now you're experienced! Unsolder the cylinders from the saddle and file the front valve detail from the steam chests, then remove the pilot, pilot deck, springs, firebox and brake detail from the frame, taking care not to dislodge the rear spacer and the ashpan. Remove the water hatch, ladder, handrails and water valves from the tender shell and the steps, rerails and brake cylinder from the tender frame. Finally, strip the smokebox front. The accompanying photograph shows what should remain.

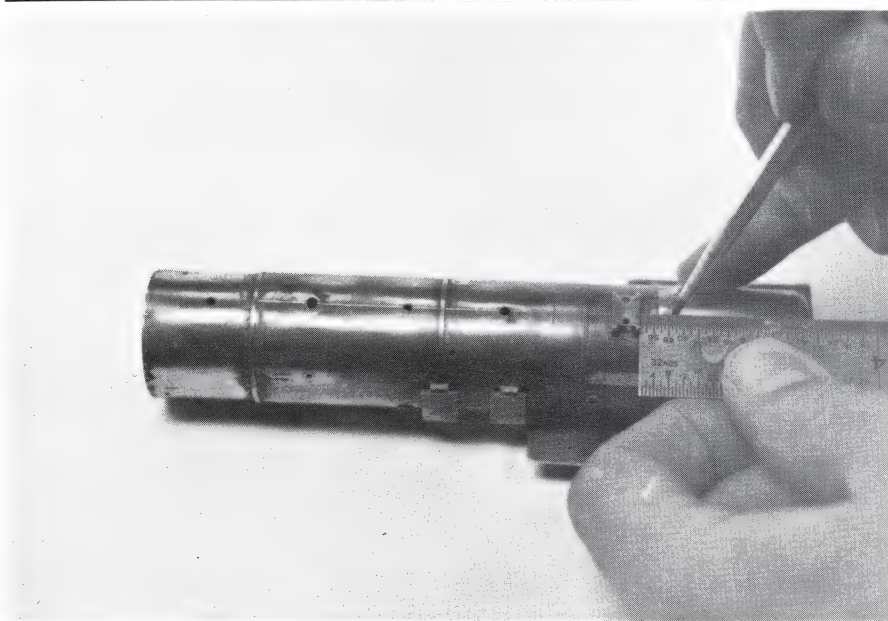


The disassembled engine showing parts kept for rebuild. The cab doors have been cut open at this time, and all excess solder removed. Though not shown, all screws and springs have been kept.

While the pieces are apart, another procedure should take place, somewhat akin to tinning only in reverse. The large globs of solder common to these models can be removed by heating them and quickly brushing with a fairly stiff bristle brush. Repeated passes may be necessary in some areas, but it beats filing all that solder off. The trick is to heat only the solder to be brushed away and then get the stuff loose before it cools. With proper timing, the solder slides free and cools before adhering somewhere else. This is aided by the fact that since no flux is present, the solder is reluctant to stick anyhow. The brush will stand the heat quite well if the flame is removed an instant before the brushing takes place. Rate yourself: Less than one brush lost — such talent! One brush — you're learning! Two brushes — back to plastic kits! Assuming things are O.K. so far — you're ready to begin assembly.



Using the wire brush and torch to remove solder.



Scribing lines on the boiler.

STEP 3: REASSEMBLY, THE SUPERSTRUCTURE

Reassembling the boiler and cab assembly is the most complicated and time consuming part of reconstruction but is also the most gratifying when completed. Before soldering anything, scribe the jacket seam lines in the appropriate locations — top center, at about 35° to either side of center top, and from the washout plugs straight back into the cab area. Do not scribe on the smokebox! Now remove any burrs that may be present. I like 4-cut needle files for this (also a jewelry supply item). Though most of you undoubtedly know correct file usage, a quick note might be in order here. File wherever possible on the forward stroke only, occasionally wiping a little chalk into the file's teeth to prevent build-up of particles between them. Clean your files often with a file card. A general rule on files: use the largest one possible for the job; it's easier

to control and gives the best finish with fewer "little" marks. If scratches occur in the filing remove with a steel or agate burnisher or 400 wet or dry paper.

Normally, it is wise to attach the largest (soldered) parts first and work to the smallest. Some exceptions are necessary, and I'll try to note these as I go along. The stack is an obvious starting point. Using a backshop casting demands enlarging the old mounting hole to accommodate the sprue which is used to cast the piece and serves as a useful mounting device. Another tool comes into play here; the micrometer. Properly used, a micrometer will save countless hours in the building of scale models. Mike the sprue on the stack and choose an appropriate drill bit of the same (or close) size. The closer the fit on all parts, the greater the accuracy and less chance for error. Where fitted joints are

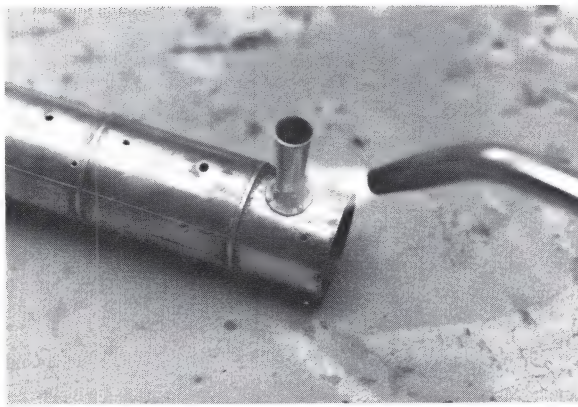
visible, such as pipes going through the cab front, closer tolerance on holes will provide a more realistic appearance. Occasionally a small reamer or cutting brooch can take an undersize hole to an exact fit.

Sweat soldering is called for on the stack here. I use 16 gauge resin core electrical solder and appropriate flux (Kester Sol. Paste, etc.). Lightly flux the bottom of the base of the stack and apply a sparing amount of solder around the bottom outer rim, having first removed the burrs and cast bubbles if present and checking the fit. There is a simple soldering technique to avoid unnecessary cleanup. Cut your solder wire into small snippets approximately 1/16" long. Pre-melt into balls and upon cooling place on the piece to be joined with tweezers or a sharpened probe. A slight buildup of flux on the

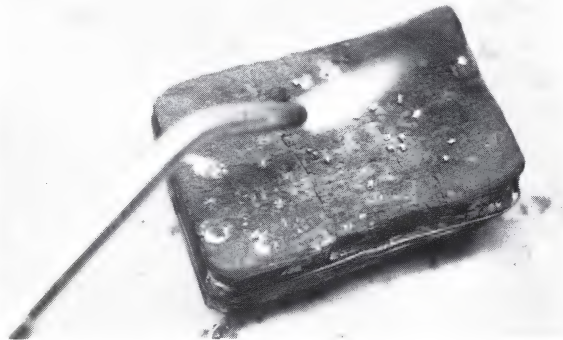
probe will aid in "wiping up" the cooled balls for placement. Then heat the piece until the solder flows, and spread the solder, if necessary, over the joint area with the probe. Coat hanger wire makes a good probe. Normally I solder on a heat proof pad (Johns-Mansville Marrimet-45) or asbestos to avoid damage to my workbench, and use a jeweler's charcoal block to melt the solder on. This block is bound tightly with soft steel wire to prevent its cracking from the heat. It may also serve as a convenient jig platform to pin parts against each other for soldering. Be sure to check under parts before heating for stray solder balls which will gleefully attach themselves to some unwanted location. Remember that the flux becomes quite liquid upon heating and unless gravity is accommodated those little balls can find their way to the wrong spot! The stack should be vertical with the bottom up. With the solder sweated to one surface (the bottom of the stack) allow the solder to flash (cool below the melting point) and then position the stack on the fluxed boiler location. Be sure the sprue is cut quite short so you can re-install the weight later. Now gently reheat the stack and smokebox inside and out until the solder shows as a thin line around the joint, and allow to cool. I should stress the importance of filing off all mold marks from each casting before assembly. Again realism is our goal and this effort can make a great difference in the overall effect.

Most of the larger cast detail can be joined to the boiler by the aforementioned technique. Again practice, beginning with the larger pieces, will help you to refine your technique by the time the smaller parts are ready to be added.

Write Kelly Morris, 632 Grant, Ft. Atkinson, WI 53538 for photo set lists and prices.



Attaching the stack by using the techniques mentioned in the article.



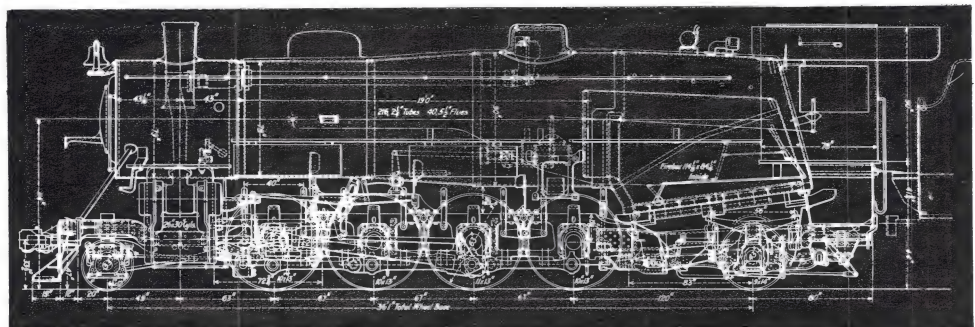
Beading up the little pieces of solder for use in attaching details to the engine.

**Key
Imports
Inc.**

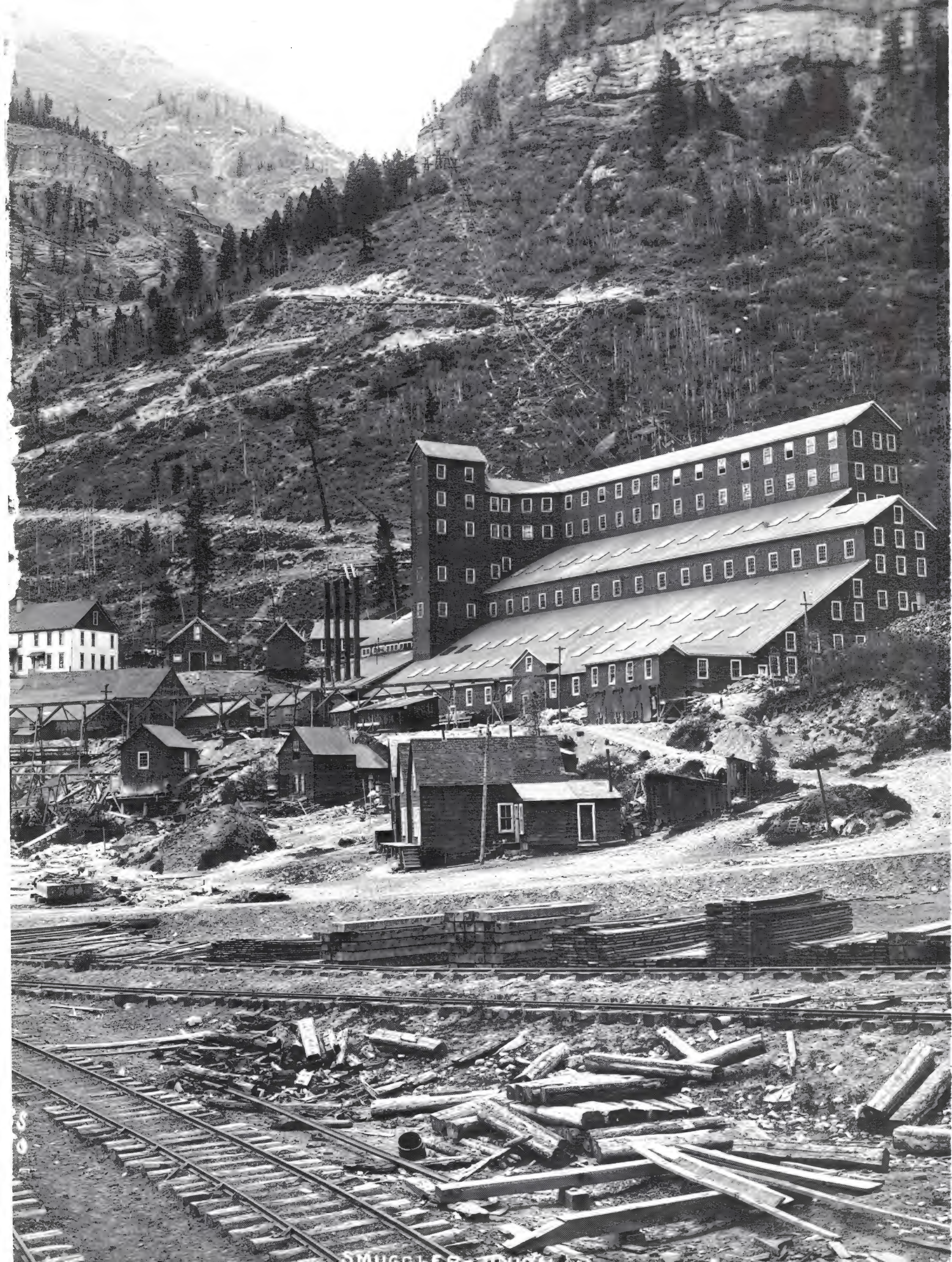
*The Key
to Quality*

655 REDWOOD AVE. No. 165
MILL VALLEY, CALIF. 94941

"BIG POWER FOR SHORTLINES & BRANCHLINES"



The USRA "Light Mike" is returning this fall. One of Key Imports' most popular locomotives, this one will be dressed up in Pennsylvania, Southern, L&N and Frisco garb.



S
O

SMUGGLER

PANDORA ABRIDGED

by F.L. Oster

Like most of us, whose ideas are bigger than our spare rooms, I wanted to build a railroad empire. For years I tried to plan layouts with the many features I wanted and still keep the thing confined to a basement. Wyes, helper grades, trestles, and the capacity for three-engine trains were all part of the plan. Eventually I was able to fit my favorite stretch of railroad (Vance Jct. to Lizard Head) in my favorite scale (HOn3) into a reasonable space (12' x 14') and encompass at least some of the things I wanted in a layout. A move in 1973 necessitated a change in thinking, as our new house had no spare room. I was forced to look elsewhere to run some track, and after giving up on using the chicken coop or tool shed, I settled on a small walk-in closet measuring only 5'6" by 4'6".

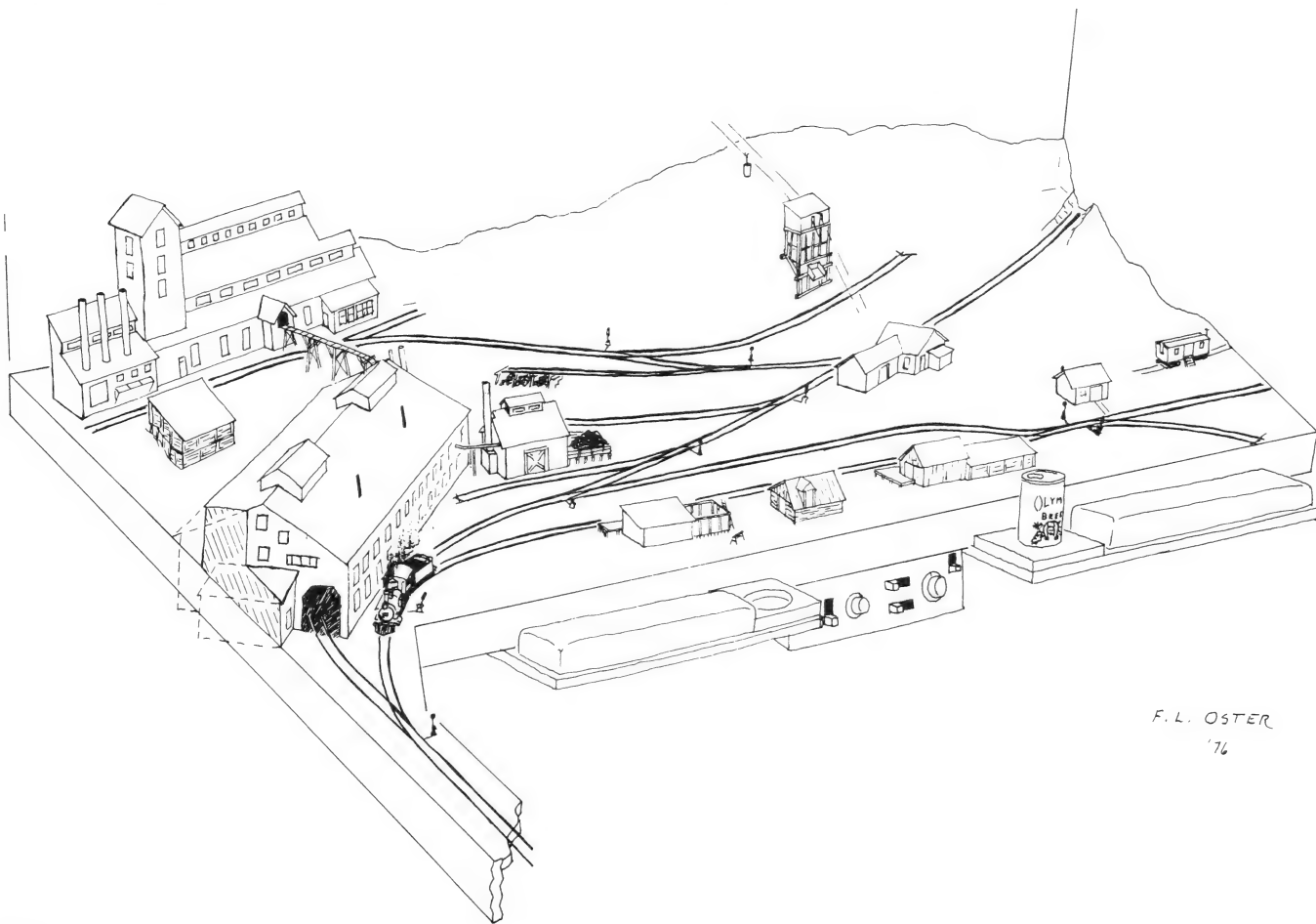
The search for a track plan was surprisingly short, I got out a pad of engineering paper from college days and started sketch-

ing. (By the way, engineering paper is very handy for all kinds of sketching as it has a grid printed on the back that shows through the front; a tablet can be bought at any store that carries drafting supplies.) Being an RGS nut for years, I had been captivated by the box canyon where Telluride is located and in particular the maze of tracks around the mills at Pandora. Two books have track plans for this yard; Josie Moore Crum's *The Rio Grande Southern Railroad* has an engineering drawing of the area made in 1921 and *Silver San Juan* by Mal Ferrell has several good pictures as well as a simplified map. The photos of Pandora are by courtesy of Pruett Publishing Company and Mal Ferrell. Quite a bit of educated guessing, imagination and selective compression were used to fit all of that track into the available space, the results are shown in the drawing and track plan.

In constructing this layout I used Lam-

bert pre-fabricated track and switches. Curves are necessarily sharp at 15-inch radius; however, I have no trouble in operation or aesthetics with equipment on these curves. The switches are modified by cutting off the long leads, leaving one inch or so behind the points and in front of the frog. This length depends on the curvature of the track leading into and away from the switch. The purpose of this is to let the curves start and end sooner in relation to the switches. Spring-loaded ground throw switch stands are used at present, as they are very positive and practical in use. Scale stands are still to be installed.

Most of the structures that I have were built with the Vance Jct.-Lizard Head layout in mind. My water tank, Lizard Head section house, and stock pens won't be used on this layout, not only because none were located at Pandora, but because they are scale buildings. Unfortunately,



◀ The Old Red 80 stamp mill as it existed around 1900. Note the clutter and the need for Grandt window castings. From *SILVER SAN JUAN* by Mallory Hope Ferrell, Copyright 1973 by Mallory Hope Ferrell. Published by Pruett Publishing Company, Boulder, Colorado. Used with permission.

▲ Though not entirely consistent with the plan, this drawing gives an idea of my present plans for scenery. The arm rests save the scenery from some knocks since rule "G" is broken frequently.

even the section house would dwarf my model of the power house, if they were in close proximity. All of the buildings for the layout are greatly compressed; the Ball Mill in scale would cover the entire existing area. Smaller structures are nearer to scale dimensions. Most buildings started as cardstock representations. These were used to get the sizing right, and as temporary scenery until the final buildings were constructed.

Most of the equipment at this time is scratch built, using Grandt parts and set in the 1903 time period. The layout is currently home to seven box cars, two coal cars, a flat car, flanger, rotary snow plow,

and business car "Rico". The power line-up consists of consolidations #40 and #15 and an F.E.D. convert 4-4-0 #36. At this writing three structures are complete and the track is operating smoothly; parts have been purchased for five more coal cars and I am eyeing another engine.

This track plan just goes to show what can be done in a small place. Sure, all of us would love to reproduce the Georgetown or Ophir loops in miniature, but even if there isn't the space for such an undertaking, almost everyone can squeeze in a well-detailed operating module like this one. One day when I rebuild the RGS in 1:87 scale, Pandora will have a prominent

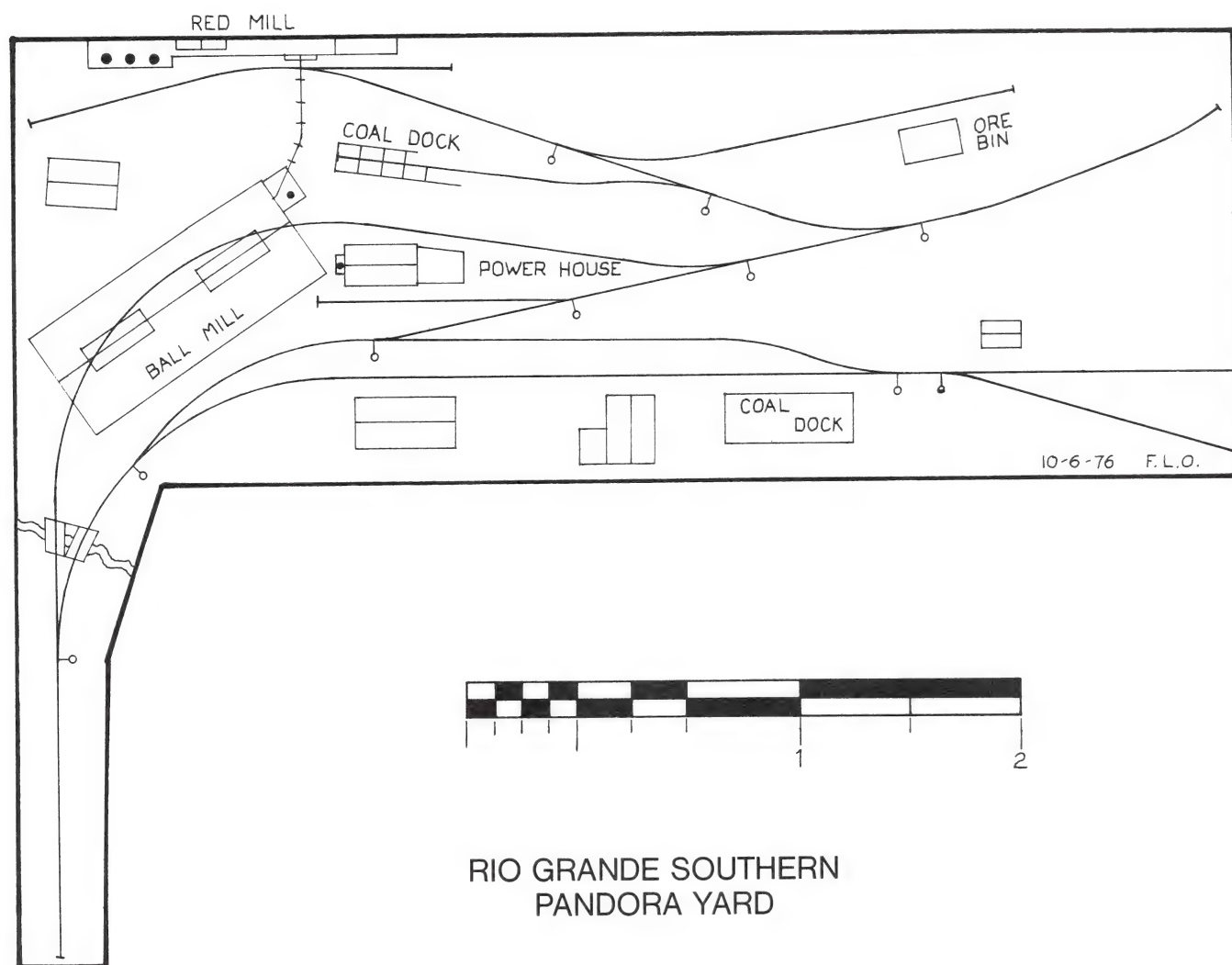
part. Until then there are still many prospects awaiting attention on my workbench, and in my sketchpad from whence all ideas originate.

References:

Crum, Josie Moore (1961). **The Rio Grande Southern Railroad**. San Juan History Inc., Durango, Colorado.

Ferrell, Mallory Hope (1973). **Silver San Juan**. Pruett Publishing Company, Boulder, Colorado.

Number six switches blend well with 15 inch radius curves. Switching leads are 18" minimum. This radius will barely accomodate a "mudhen" and two cars. Foam rubber is used at the end of the track to protect the equipment from highballers.





◀ The original Tomboy mill and mine circa 1889. From *SILVER SAN JUAN* by Mallory Hope Ferrell, copyright 1973 by Mallory Hope Ferrell. Published by Pruett Publishing Company, Boulder, Colorado. Used with permission.

▼ The men who made it all happen, a study in mining costumes. Tomboy is in the background. From *SILVER SAN JUAN* by Mallory Hope Ferrell, Copyright 1973 by Mallory Hope Ferrell. Published by Pruett Publishing Company, Boulder, Colorado. Used with permission.



RGS #74 at Pandora, Colorado on June 1, 1949. From *SILVER SAN JUAN* by Mallory Hope Ferrell, copyright 1973 by Mallory Hope Ferrell. Published by Pruett Publishing Company, Boulder, Colorado. Used with permission.

The MODELMAKERS NOTEBOOK



by
AL ARMITAGE

by Al Armitage
Photos by the author

SOMETHING DIFFERENT IN ROOFS.

It always strikes me as strange that few modelers ever use anything other than wood shingles or tar paper for roofs on their buildings. During the periods that most of us prefer to model, there were any number of other materials commonly used for roofing, which offer a variety of designs, textures, and "characters" to offset the repetitious "sameness" of wood shingles and tar paper.

On most model railroads, we see much more of the roofs of buildings than is normal in real life, thus this part of a building is an important design feature of the model scene. Consideration should be given to which type of roof will look the best in contrast to others nearby.

As a case in point, study the photo of the display diorama I recently built for Precision Scale Company. Both the water tank and the hand car shed have shingle roofs. The sand house and the pump house, under the oil tank, have tar paper roofs as do the two fire hose sheds. When it came to designing the depot, I wanted something else for variety and, as the building sits by itself away from other structures, it needed to stand out so it would not be overbalanced by the related

group of structures at the other end. In this particular building, the roof, with its wide overhang, is the most visible, and thus the most impressive, part of the structure so I decided to emphasize that feature. I chose a weathered copper roof oxidized to a light green color which would make a pleasing combination with the buff and brown trim color scheme of the station. Even though the building is small (15 x 30 foot size) it is spectacular enough to hold its own against the larger buildings behind it. Judging from the comments at the PCR convention in Phoenix, the concept worked as I had hoped it would. The depot drew a great deal of interest and not a few compliments.

Metal roofs are very easy to model, especially if you work in styrene. Several different metals were used for roofing and the same basic construction principles apply to nearly all except for the corrugated type. The drawings show the general features of construction along with a variety of seams used. The standing seam is shown in its open form. In actual use, it would be crimped tight. The flat seams, similarly, would be hammered flat and, on low pitch roofs, would be soldered, as

well. On a model, the seams can be made of strips, as shown. Flat seams can be indicated with a scribed line. The joints of a ribbed seam would be crimped tight, also. Treatment of hip angles, for both types of seams, is shown as well as two styles of ridge design.

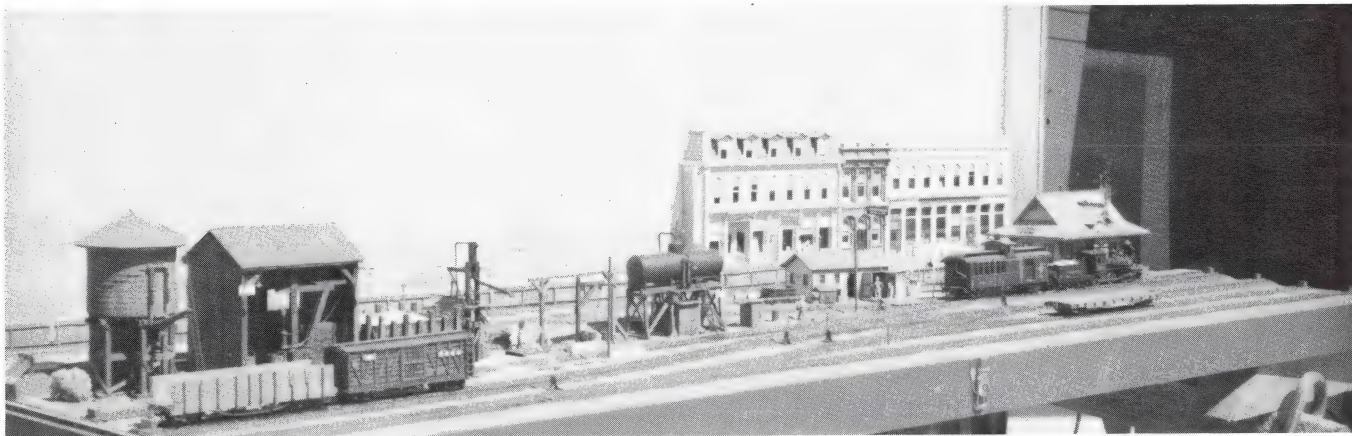
The spacing of the seams is governed by the sheet width in all cases. The 1 inch high standing seam subtracts $2\frac{3}{4}$ inches from the width of a sheet, for instance, while the 2 x 2 inch batten seam reduces the width of a sheet by 3 inches. This must be taken into account when you lay out the roof for a given building. Choose the width that results in the closest to equal spacing for the length of the roof. The table lists the stock sizes for each particular metal and the size most commonly used. However, pick the size which best fits *your* situation. Small structures would employ smaller sizes, larger ones are better suited to large buildings.

If you are not familiar with the name "terne plate" it is pronounced "turn" and is a thin steel sheet with a slightly brownish-silvery color. It was the forerunner of the more recognizable "tin plate" which had a coating of tin and lead which made it shiny. Both metals were used for all sorts of tinware, furnace ducting, tin cans, as well as roofing material.

Another type of metal roofing was galvanized iron which was manufactured in sheets with the seam ready formed. These sheets were 24 inches wide and 5 feet to 12 feet in length. They were zinc coated to prevent rusting and were usually painted for further protection. Most corrugated sheets were made of galvanized iron.

It is interesting to note that zinc roofs, according to recommendations, should be allowed to "weather" for at least two months prior to being painted. Presumably, this allowed the shiny surface to oxidize enough to give the paint something to hold on to. Zinc should not be used in contact with iron, steel, or copper because of electrolysis, nor with oak, redwood, or cedar.

Zinc expands $2\frac{1}{2}$ inches per 100 feet when the temperature changes from 0° to



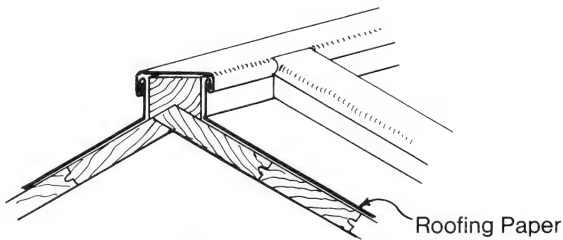
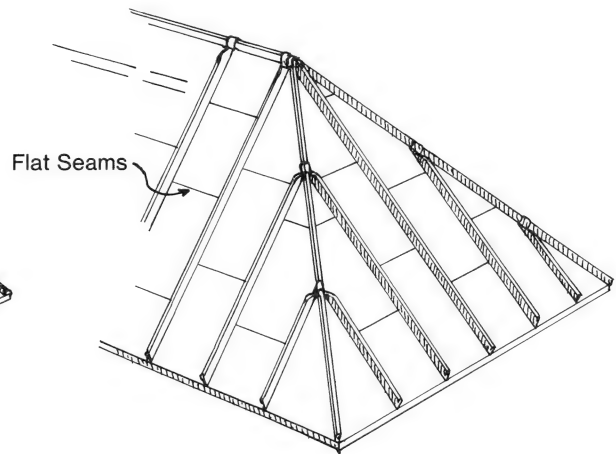
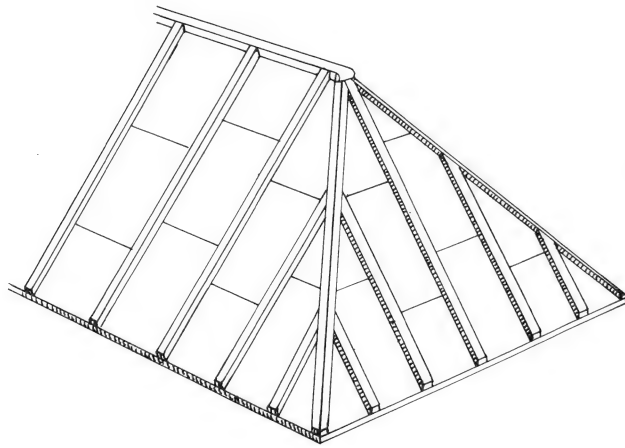
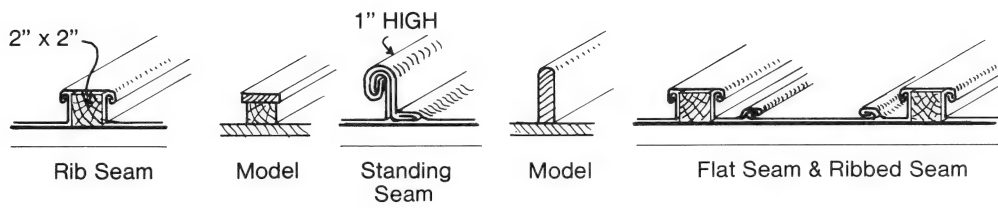
The Precision Scale Company diorama with its variety of roof types. It is 8 feet long by 30 inches wide.



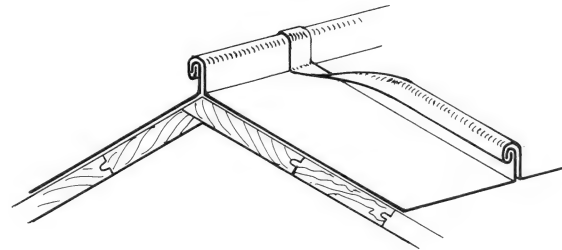
The depot with copper roof, fancy ridgepoles, and lightning rods. The design is free-lance but the paneling is similar to my old "Tomahawk" station.



This old school building is in Edna, California. Note the flat metal sheets used as siding – obviously a repair job.



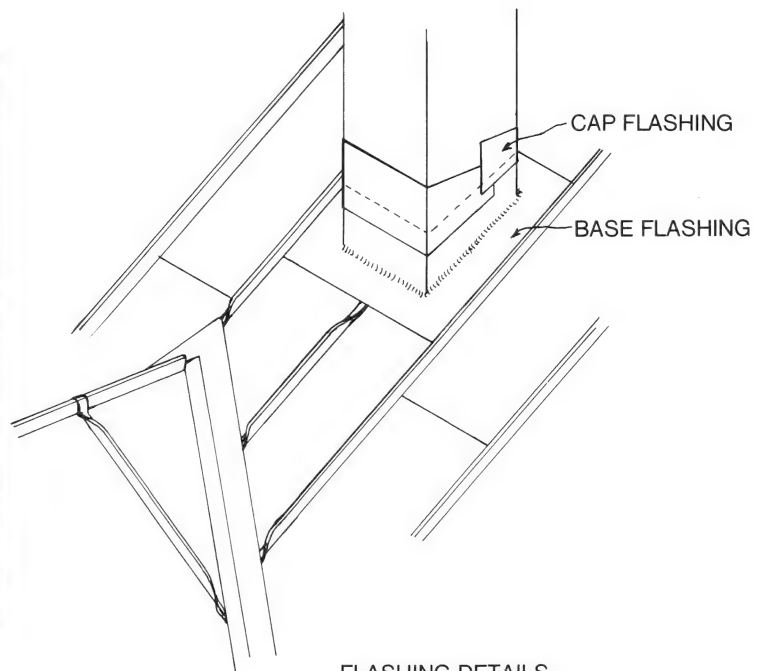
BATTEN or RIBBED SEAM ROOF



STANDING SEAM ROOF
(For pitch 3' to 12")



A typical example of corrugated siding. Note the variety of faded lettering. Try this on your models.



FLASHING DETAILS

120° degrees F. Copper expands about half that amount. Tin about ⅓ inch per 100 feet.

One of the nicest things from the modelers standpoint about metal roofs is the wide variety of coloration possible with their use. A copper roof, for example, depending on its age, can be painted to look like it was fairly new (the color of a new penny) or in varying tones of brown (like an old penny) as well as several shades of verdigris (green). This allows you to choose the color best suited to fit into the surrounding tones, and textures, of your particular situation.

If the copper roof colors don't work out for you, then consider a tin roof or a zinc roof. Tin roofs were usually painted with red lead or iron oxide coatings to prevent rust. However, unless they were repainted every two years or so, the tin would rust so you have a choice of color variations here. Zinc did not rust, of course, but it did oxidize to a dull gray, if not painted and, in time, often acquired a whitish powdery look mixed with the gray in much the same manner as the verdigris on copper roofs. Zinc roofed buildings near salt water especially showed this effect. New zinc is silvery gray and shiny, and difficult to model effectively.

Around the turn of the century, metal roofs were a common sight on schools, churches, and industrial buildings. They were also popular for larger residences, especially those with mansard type roofs.

Although you don't need to know this to model a metal roof, you might find it interesting that the sheets were held in place with cleats which were nailed to wood sheathing. The metal sheets themselves were never nailed directly as this would have created a potential leak point.

The familiar corrugated siding is a more recent development that was used for initial construction as well as for covering old roofs, etc. Its design gives it self-supporting rigidity making the material ideal for siding on all sizes of industrial structures. Usually applied to a metal framework, the sheets were laid with a two corrugation overlap for roofing and a one corrugation overlap for siding. In new construction, sheets were fastened with nuts and bolts. They were usually nailed in place over existing wood construction.

Here, again, the color variations possible are legion. Some corrugated buildings were painted but most were not. In time, the galvanized coating weathered off allowing the steel base to rust. This generally occurred first on the ridges giving a unique striped effect not found in other types of metal construction. Modeling it can be fun and provide a much needed variation in the color scheme of your scenery.

Next issue, we'll talk about some other rarely modeled roof types. See you then.

AL —

SPECIFICATIONS OF METAL ROOFING SHEETS

TERNE PLATE: Stock sizes

30 gauge (.012")

14 x 20 20 x 28 14 x 96 24 x 96 28 x 96 30 x 96

28 gauge (.015")

10 x 96 14 x 96 20 x 96 24 x 96 28 x 96 30 x 96 28 x 120 30 x 120

Commonly used sheet = 20 x 28

COPPER:

Sheets in multiples of 2" (width) x 8 ft. in length

24" width commonly used

ZINC: Stock sizes

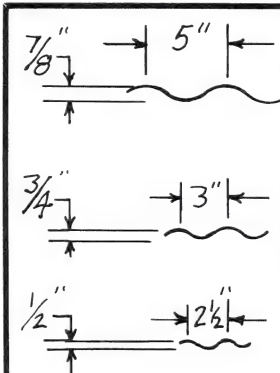
20 30 36 40 inches wide x 7 and 8 ft. long

GALVANIZED IRON:

Sheets with seam ready formed 24" wide x 5' to 12' lengths

CORRUGATED IRON (Galvanized)

Sheets 26" wide x 5' to 12' lengths



Standard corrugations ►



A brick house in Austin, Nevada with ribbed seam roof.

SLIM GAUGE SUPPLIES



REED'S HOBBY SHOP

8230 La Mesa Boulevard
La Mesa, California 92041

714-464-1672

The Silverton Northern Railbus in Brass

by Bruce Eaton
Photographs by the author

When I decided I wanted a Casey Jones, as the Silverton Northern Railbus is affectionately called, I decided to build it in brass. Why? There is a beautiful model Casey available from Scale Structures Ltd. in pewter, why not just buy it? The only answer I can give is: "I like to work in brass and recycled beer steins just don't appeal to me."

I set to work with a set of Sloan etchings, their accompanying instructions and plans and photos of Casey from Sloan and Skowronski's book, *Railbow Route* (Sundance, 1975). The first thing I noticed was that the etchings and the plans treat the windows as if they were all separate and equally spaced. So, I observe from photos, does the SS Ltd. model. The photographs of Casey, however, show that the back four windows are paired off rather than equally spaced. Moreover, the photographs indicate that the exhaust pipe is on the opposite side to that indicated on the plan.

Generally, I followed the instructions that come with professor Sloan's kit, however, I did a number of things differently or in addition which I will describe here. The first and most tedious part was removing the material from all those windows. At Sloan's suggestion I milled out most of the material using a dental drill as a small end mill (Photo 1). This reduced the amount of material that had to be removed with the file. In order to insure uniformity of size and curvature I clamped the sides together, and then the front and back together, when filing the edges. I decided that it would be very difficult to bend the cowl to the proper flair. Instead, I opted to file the motor compartment and cowl out of a solid piece of brass, and then wrap the hood around it. When filing this piece be sure to take into account the thickness of the brass hood wrapper.

When soldering the sides and roof together I use many small electrical alligator and flat-nose clips. These are available from your local electronics supply house.

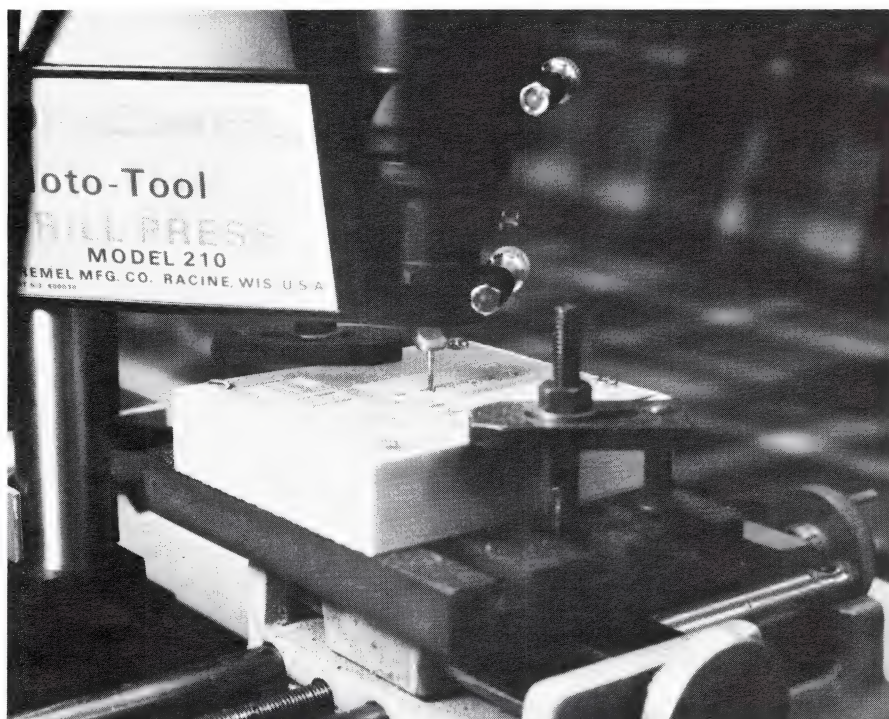
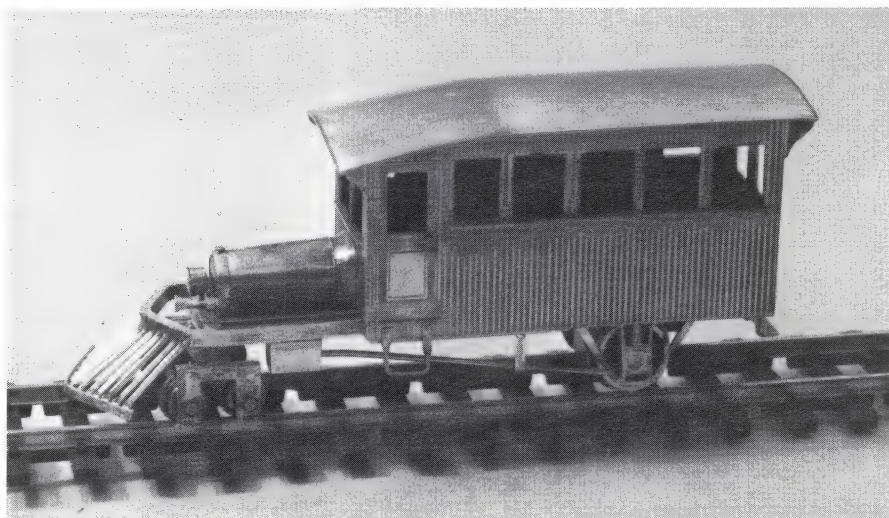


Photo 1. Cutting out the window and doors from Sloan's Casey Jones etchings. A Dremel drill press is being used as a milling machine.

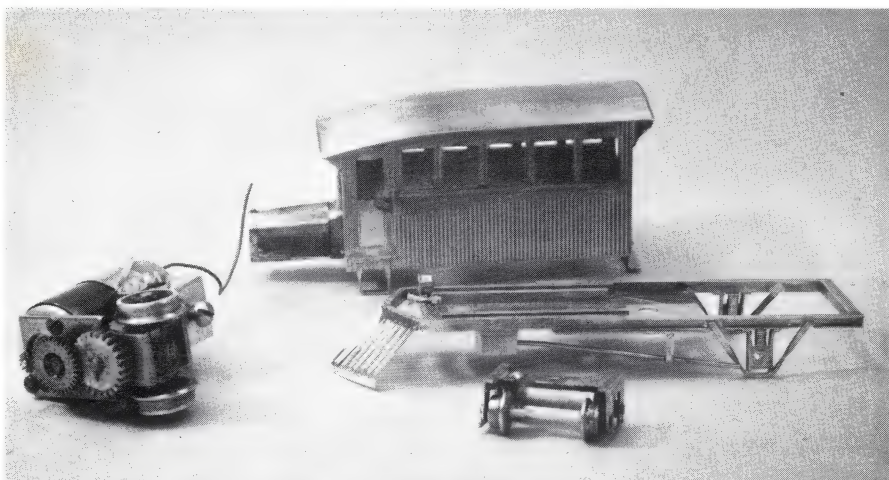


Photo 2. Casey Jones body, frame, modified Flea, and lead truck assembly prior to final assembly.

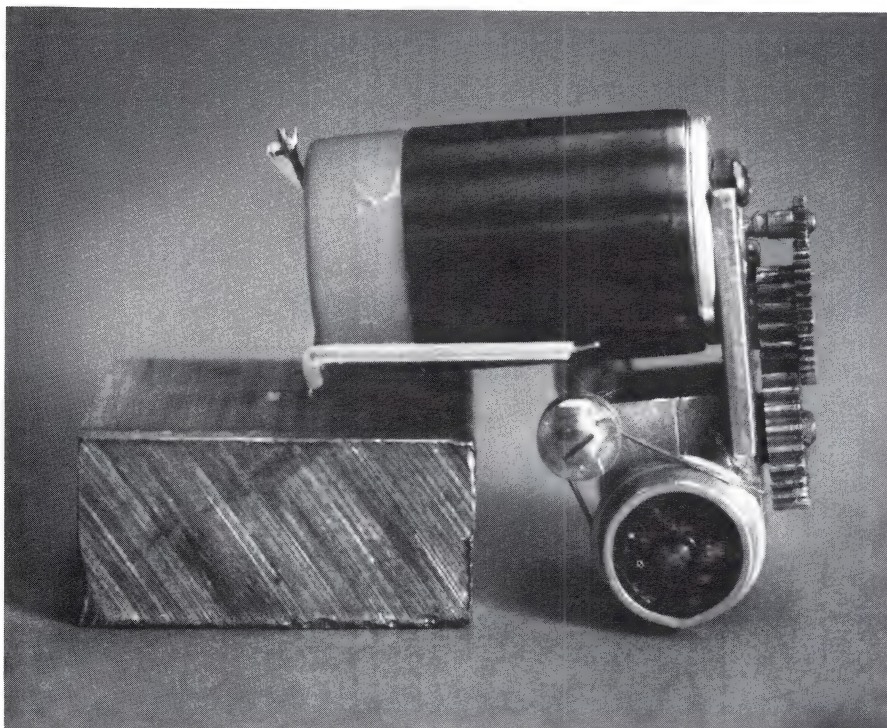


Photo 3. Modified Flea motor unit used in the Casey.

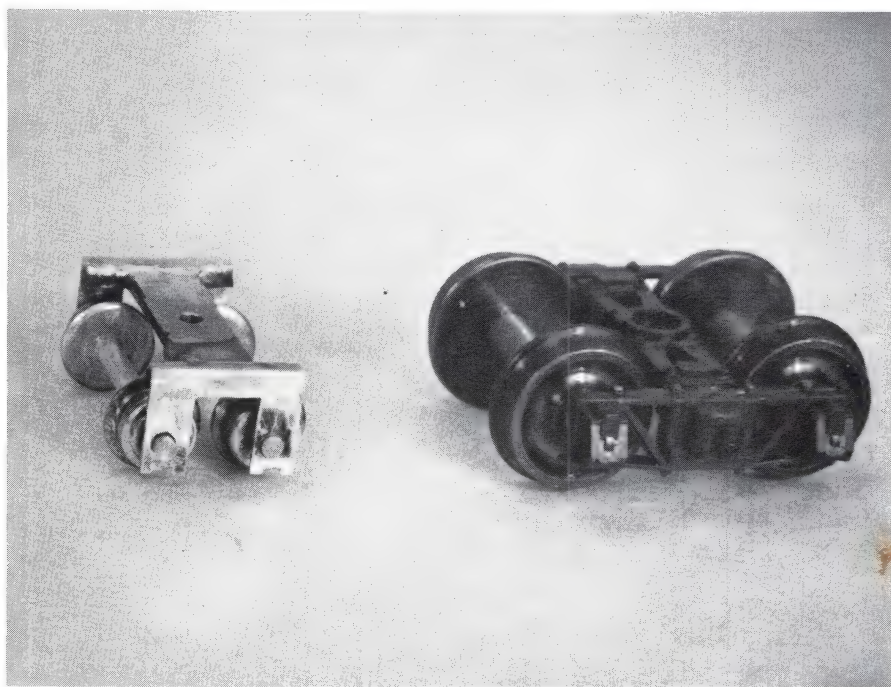


Photo 4. The Casey Jones lead truck next to an HOn3 freight truck.

Occasionally one of these clips will get soldered to the piece, but with a little gentle bending they can be removed without damage.

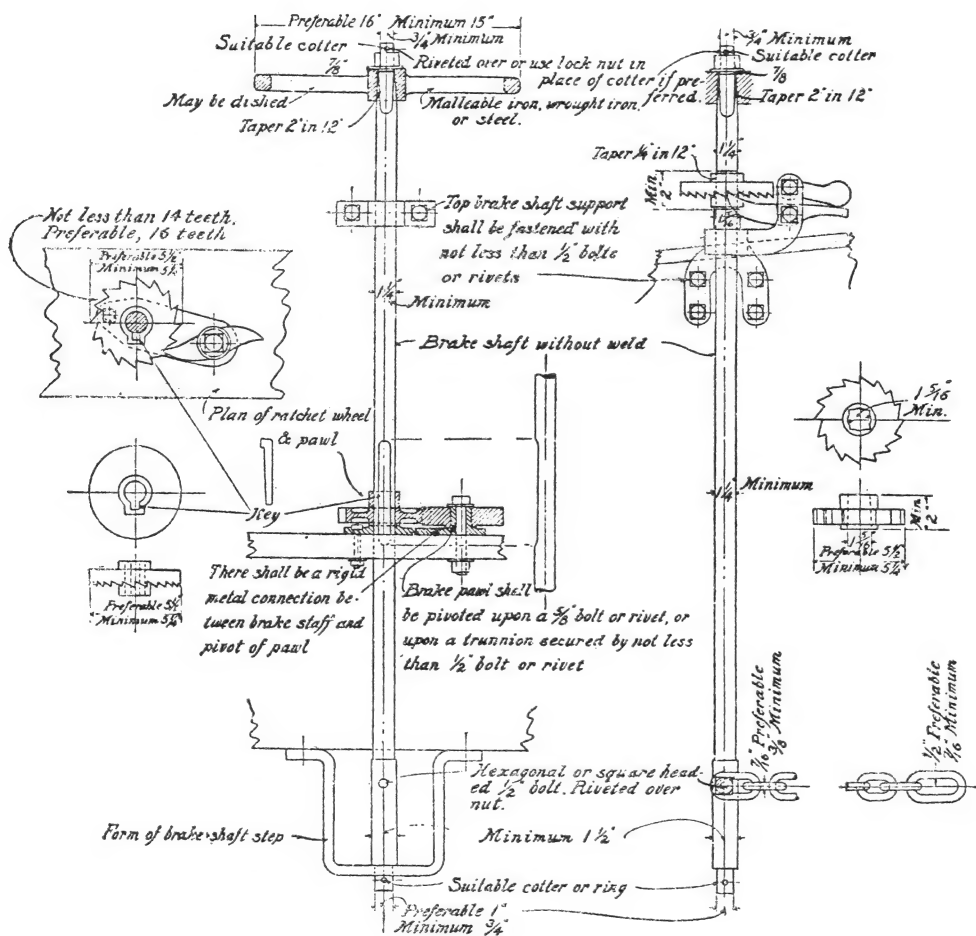
Instead of using needle files, to file the roof smooth after shaping, I used a flat, $\frac{3}{4}$ inch-wide number 4 file. I found that the

smaller files tended to accentuate some indentations instead of removing them. After sanding the roof with emery cloth, I polished it with a rubber polishing wheel in a Dremel tool. I decided to add sills to the windows, and doors, but had no $\frac{1}{64}$ by $\frac{1}{64}$ inch stock, so I had to file down

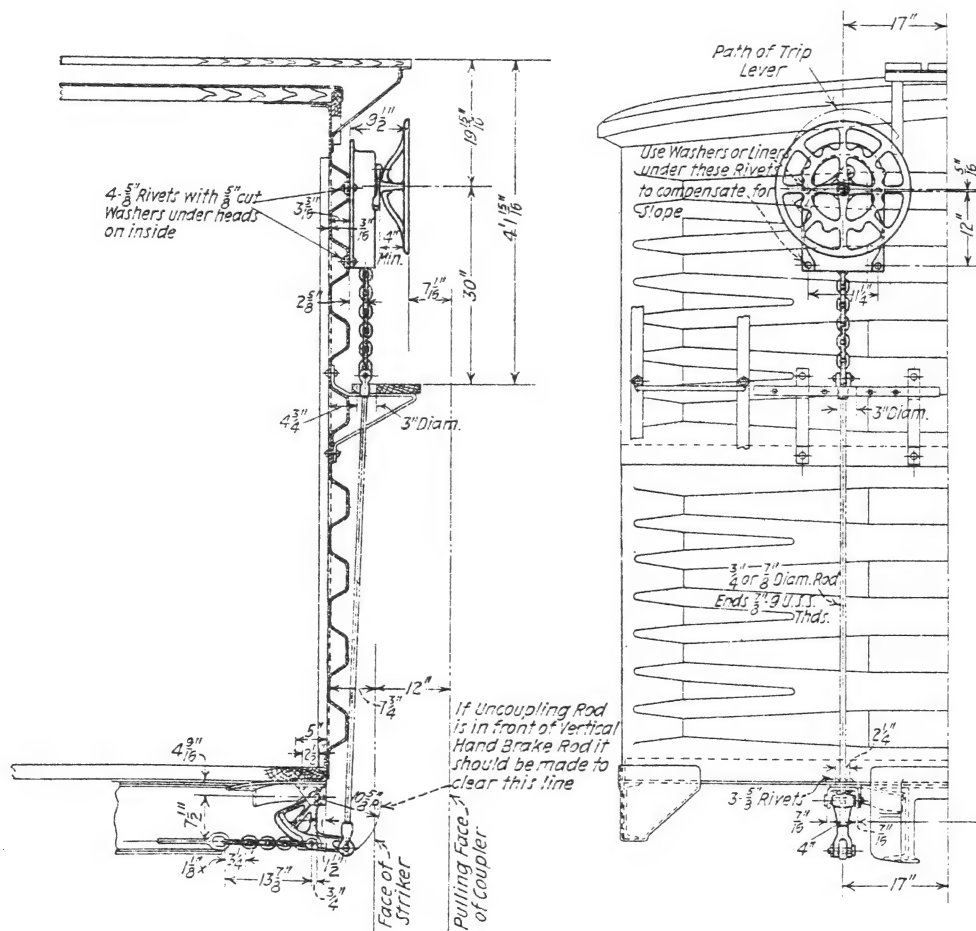
$\frac{1}{64}$ by $\frac{1}{32}$ inch stock; the frame is made from $\frac{1}{16}$ by $\frac{1}{32}$ inch stock, $\frac{1}{16}$ by $\frac{1}{32}$ inch channel and other size material (Photo 2). The original "Casey" had a pilot constructed of angle iron, so I decided to make one. I used Simpson's 3 inch by .006 inch strips and shaped them on a lead block by laying a knife down the center and rapping it with a hammer. I would not do it this way again because the pilot lost the delicate look of the original. I would use fine wire instead. The headlight was turned on a Unimat, and the horn was cut from a diesel horn cluster.

The drive mechanism started life as a Northwest Short line, 12 x 20mm motor Flea unit. While the Sagami motors are a tremendous improvement over the old open frame motors, the start-up characteristics are better with a coreless, or true can type motor than with the Sagami. I decided to replace the Sagami motor with a 12 x 20mm Maxon motor, and to increase the gear reduction. The gears (Photo 3) are from an old pocket watch. The small gear is secured to the motor shaft with Hot-Stuff, and the larger gear is soldered to the original intermediate gear. It is very important that these latter two gears end up concentric after they are soldered together. To insure alignment I first machined an aluminum jig for use when clamping the two gears together for soldering. The original wheels were replaced with plastic centered, spoked wheels from Kemtron. Unfortunately, the tires are brass and, when they get dirty, cause electrical problems. If I were doing it again I would machine tires from nickel silver.

I felt that two-wheel electrical pick-up was risky at best, and to me operationally not acceptable. I decided to try to make a lead truck which would give me electrical pick-up from both sides of the track. The small truck shown in Photo 4 is the result of those efforts. For purposes of comparison I included a Grandt D&RG freight truck. The lead truck wheels were machined from nickel silver. Short sections of steel wire were soldered into the wheels on the outside, extending only halfway through the wheel. The axles between the wheels were fabricated from insulated material to provide the correct spacing and electrical separation. With this arrangement each side frame is electrically connected to its respective track. For a bolster I used a piece of double sided, $\frac{1}{32}$ inch thick printed circuit board. A break was etched in the copper on both sides between the center hole and one end. The spring and bolster screw provided electrical contact to one side and a wire soldered to the bolster provided contact with the other side. This method of providing extra electrical pickup worked, and provided six-wheel pickup for what might otherwise be only two-wheel pickup.



Standard and Safety Appliances for Brake Wheel, Shaft, Etc.



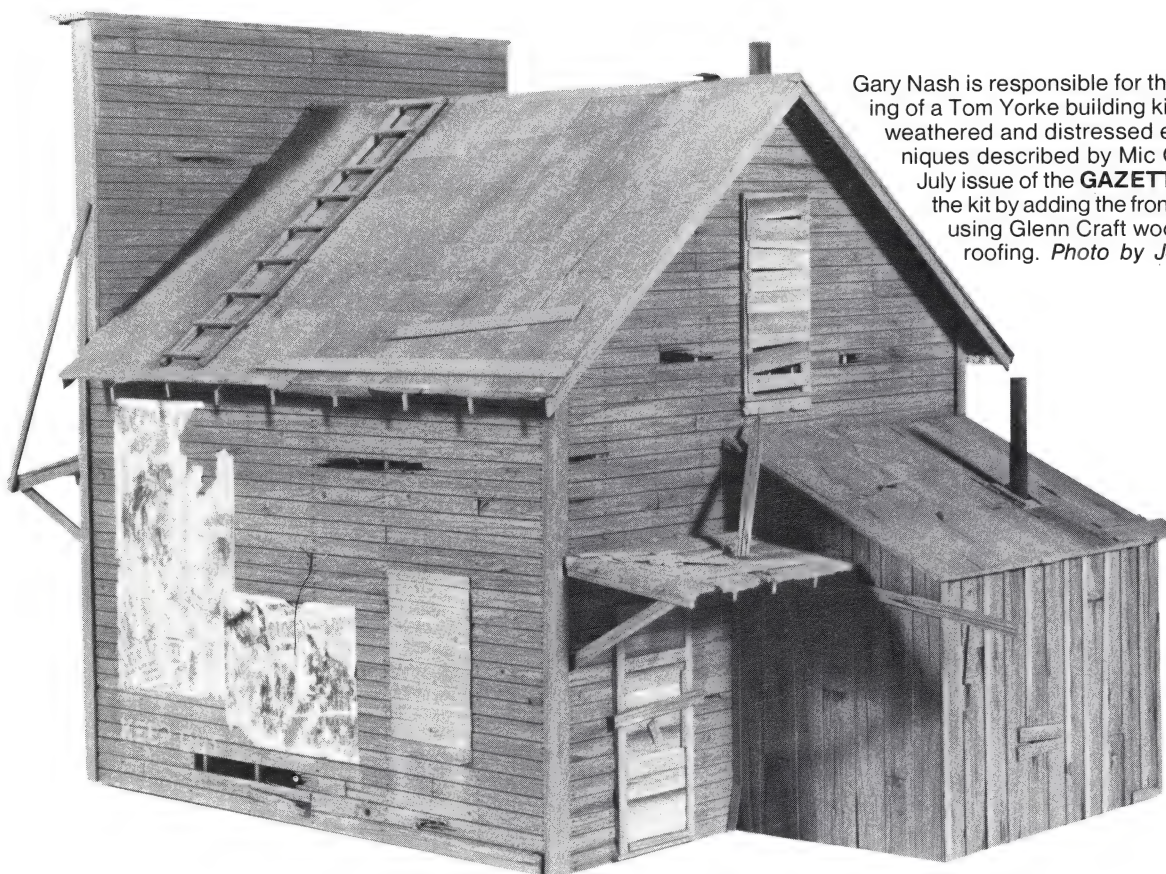
Application of Ajax Hand Brake to Box Cars. Ajax Hand Brake Company.



J.L. Barnard built this HO scale model long before Durango Press brought out their kit. He used sheet brass formed over a bottle to create the blade housing. The blades were cut from sheet plastic. The car body was made from wood and the roof was covered with 600 grit sandpaper. Brass was used to construct the tender. The builder's effort has resulted in a very realistic model. *Photo by J.L. Barnard.*

GAZETTE GALLERY

Edited by Gene Deimling

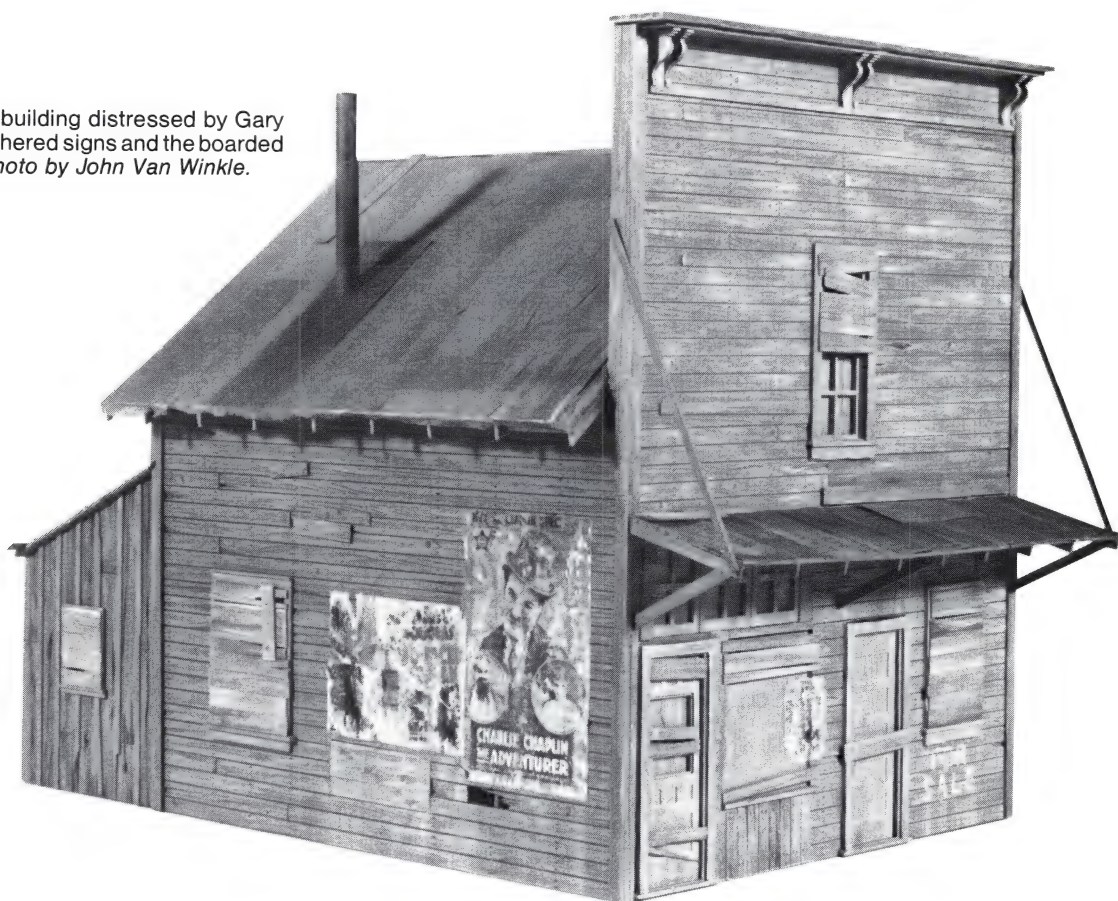


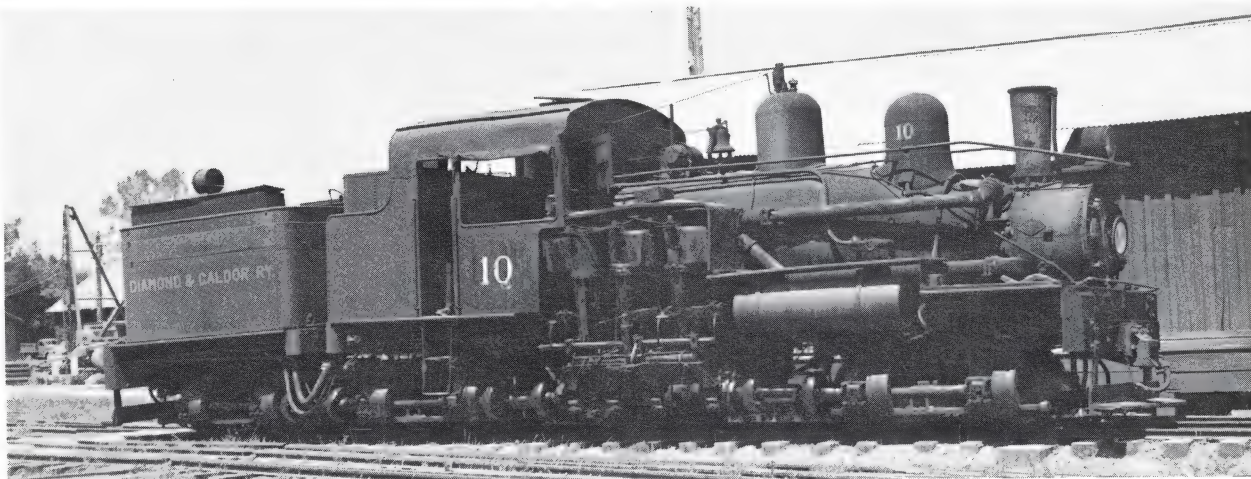
Gary Nash is responsible for this artistic rendering of a Tom Yorke building kit. He created the weathered and distressed effect using techniques described by Mic Greenberg in the July issue of the **GAZETTE**. Gary modified the kit by adding the front and rear canopy using Glenn Craft wood and Campbell roofing. *Photo by John Van Winkle.*



This close up photo of the rear wall of this building built by Gary Nash shows the incredible effects added to the wood. The knots, split battens, and nail holes add character to this structure. *Photo by John Van Winkle.*

A front view of the Yorke building distressed by Gary Nash. Note the use of weathered signs and the boarded up doors and windows. *Photo by John Van Winkle.*





The Diamond & Caldor was the next-to-last narrow gauge logging show. Time was growing short for this line and this hefty Shay when this photo was taken. Charlie Givens was at Diamond Springs, California in May, 1953, to record this locomotive's last days. *Photo by Charles Givens.*

Almost half-a-century earlier than the Diamond & Caldor photo (above) and 3000 miles away, this scene of Goodyear Lumber was recorded by an unknown photographer. Goodyear had an extensive operation in Pennsylvania early in this century. Two Shays and loaders make up this posed shot. *Photo, Collection of H.W. Matteson, restored by Bill Glass.*



The "Slim Princess" at speed approaching Owenyo, California on July 17, 1959. The Southern Pacific's contribution to the world of narrow gauge has not fared too well with model railroaders. The railroad had a great deal of charm but has failed to attract sufficient followers to permit new production of models. *Photo by Bob Trenner.*

RIO GRANDE



dry transfers -

0404 0404

by vintage Reproductions

two cars-styles per set

HO - N 1.25 O-S 1.75

Box 7098 Colorado Springs, CO 80933




The Structure Company

an SS&AE brings our brochures.

HO scale



Box 6375 Washington Station
Reno, Nevada, 89513

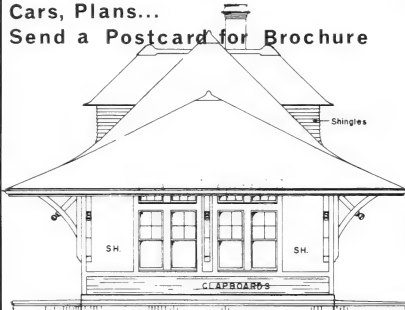


E.S.M.

1/4"=1'0" (215) 865-4791
4075 POND CIRCLE
BETHLEHEM, PA 18017

Balboa	D & RGW K-36 coreless motor	500
Iron Horse	D & RGW K-27 #461	1095
Iron Horse	Mich Cal #11 Shay	598
Iron Horse	Baldwin 2-8-0	900
Westside	NYC J3a Superdetail	695
PFM	D & RGW T-12	575
Durango Press	RGS Rotary #2 kit	239
Sunset	D & RGW K-28	600
Backshop and Precision Parts		

Cars, Plans...
Send a Postcard for Brochure



GREEN LANTERN MODELS
BOX 1605, MANASSAS, VA 22110

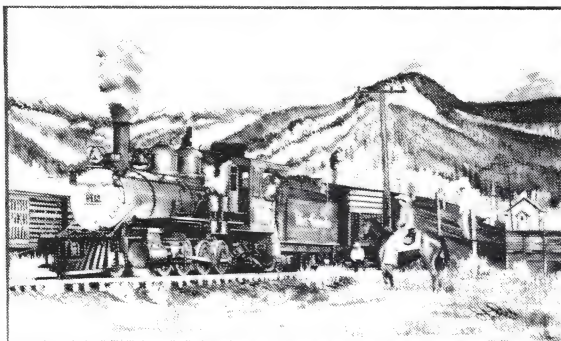
Watercolor Originals by Jan Rons

Sensitive, beautifully rendered, authentically detailed railart with emphasis on narrow gauge and short line subjects.


Brochure available on request from:

RON'S ART GRAPHICS

4045 E. Palm Lane
Phoenix, Az. 85008



ENGINEER, G-4



PORTER, G-6



Gilpin Tram Line

FINESCALE 'O' GAUGE FIGURES

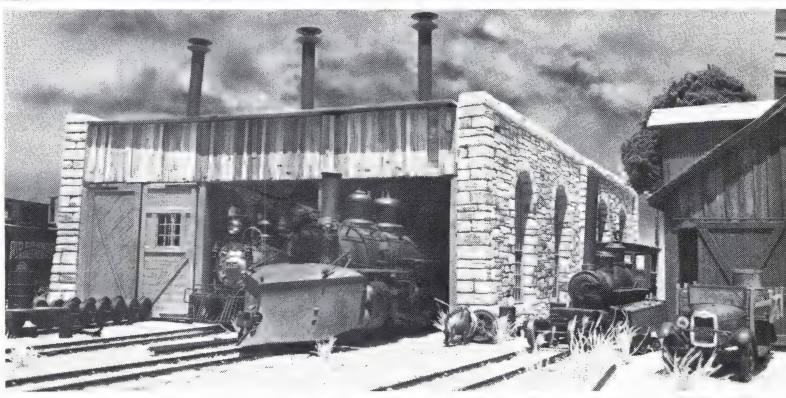
Unpainted. \$2.00 each. Painted \$3.50. Complete line available. Send for list. Send check or money order with order. Include \$1.00 P. & H.

GILPIN TRAM LINE, 213 E. 55th St.
Downers Grove, IL 60515

the ROUNDHOUSE

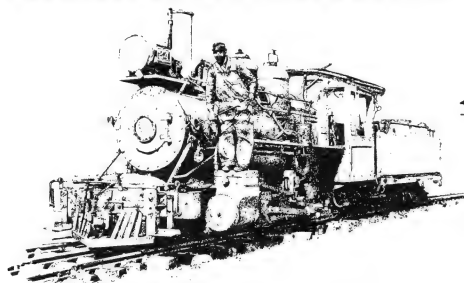
Our basic 3 stall kit, ready in November, will fit three K-28's. Send \$1.00 for THE SHOPS Catalog for the full scoop on this and other kits in this O scale series.

Thomas A. Yorke Ent.
P.O. Box 34
Sunland, CA 91040



Locomotives custom painted by Mason Locomotive Works.

NOTES FOR THE EXTREMELY NARROW-MINDED

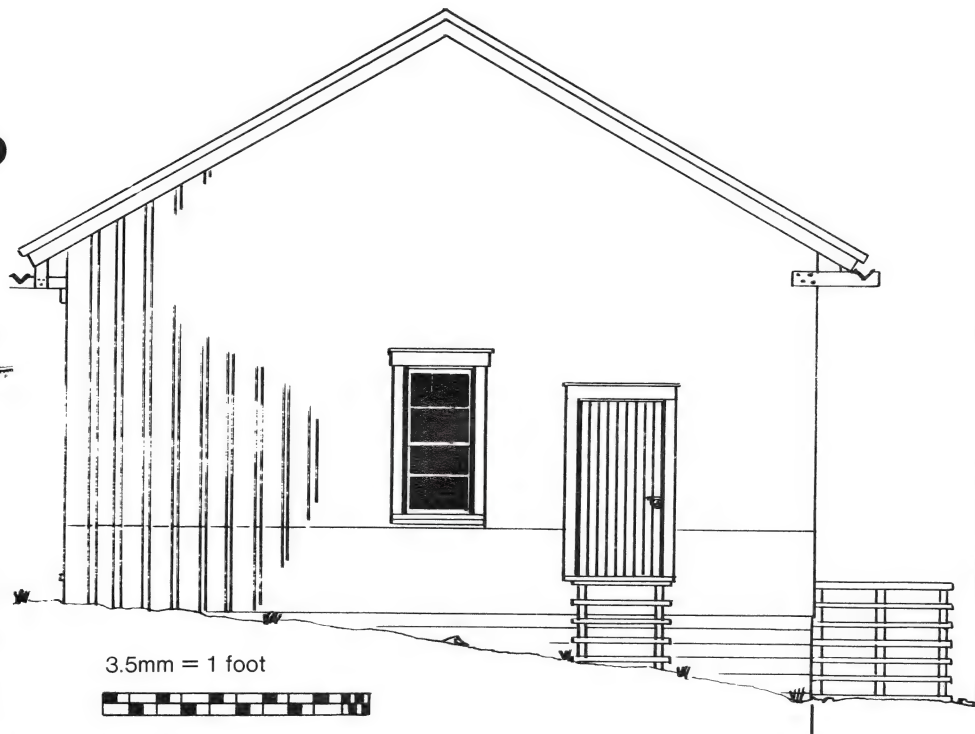


by Bill Jensen

As I promised some time ago, in this issue I'll take you north into the area of Maine served by the Monson RR. On this visit to America's only two-foot slate hauler we'll spend our time at Monson Junction, the place where the Monson RR met the mainline tracks of the Bangor & Aroostook RR (BAR).

First let's review the history of the Monson RR. The company was organized in November, 1882, as the result of efforts by Slate mine owners. As soon as enough investors were convinced to invest, building began. A six mile line was constructed south from Monson Village to a point one mile west of Abbott Village; this point was called Monson Junction. There the Monson RR connected with the Bangor & Piscataquis, built only a short time before. Later the B&P became the BAR. By 1886 the owners were planning to extend the Monson RR south out of Monson Junction to connect with the then northward building Seabasticook and Moosehead RR. The owners were also planning to extend the Monson northward about 12 miles to connect with the Canadian Pacific RR at Greenville Junction. If either of these plans had been carried out the line would have been standard gauged. Fortunately for narrow gauge fans (and unfortunately for the owners) neither plan was carried out. Without these extensions the Monson RR piled up one of the largest deficits of any narrow gauge line then in operation, over \$160,000. Not much money by today's standards but in 1890 it was comparable to the national debt. Eventually the Monson Slate Company assumed control of the railroad and operated it as an adjunct to its slate operations.

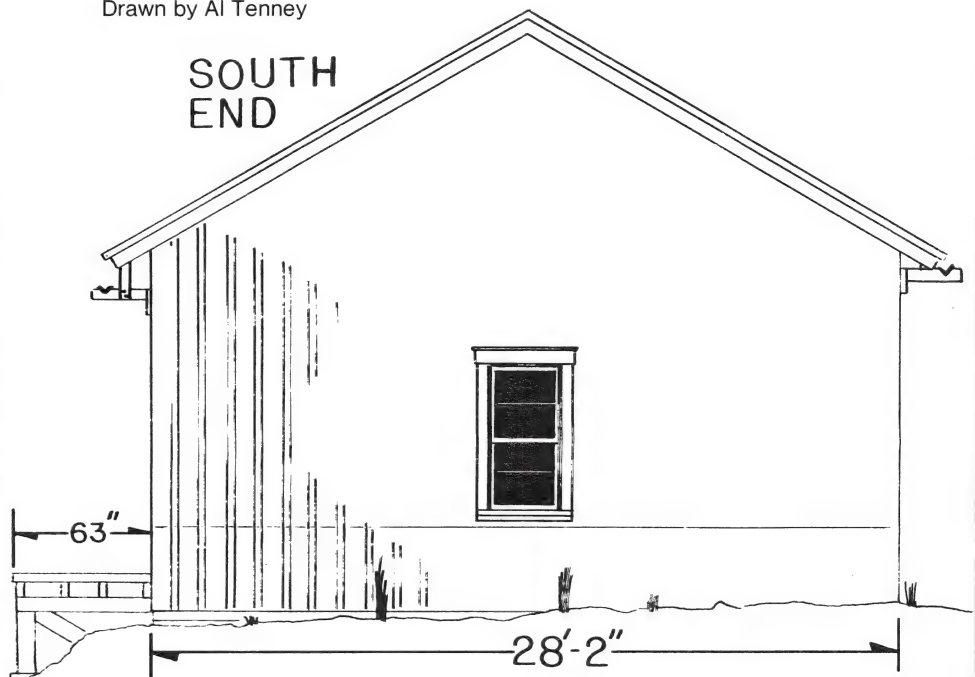
The Monson Slate Company was good, if a little stingy, for the railroad. New cars were added to the roster and the earlier engines were eventually replaced with two heavy Vulcans. The line even began to show some profits. Rather than an industrial RR, I prefer to think of the Monson RR as a typical common carrier which also served a single industry. The major freight carried was slate products to the junction



3.5mm = 1 foot



Drawn by Al Tenney

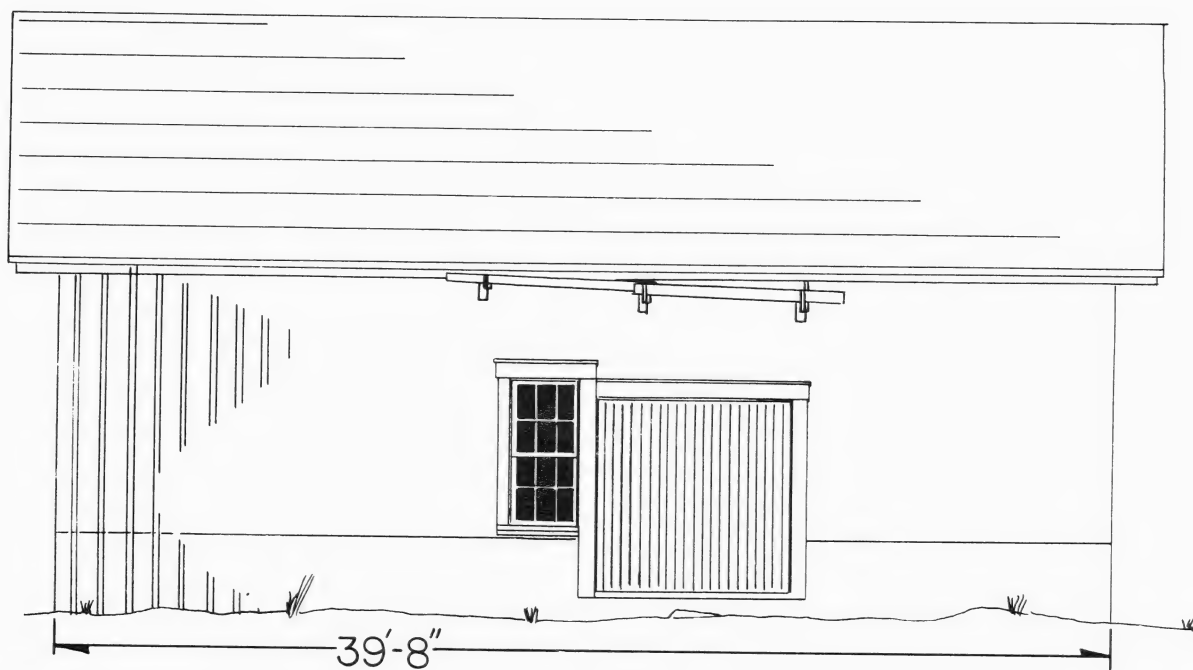


and coal and sand for polishing to Monson. Wood, used to crate the slate, was also a large traffic item. Slate is a heavy commodity and chips readily when handled too roughly. Slate trains moved slowly, hauling shipments to the junction. At the junction, the freight shed was a hub of activity as transfer crews used two wheel dollies to unload the narrow gauge flatcars and load the standard gauge cars on the other side of the building.

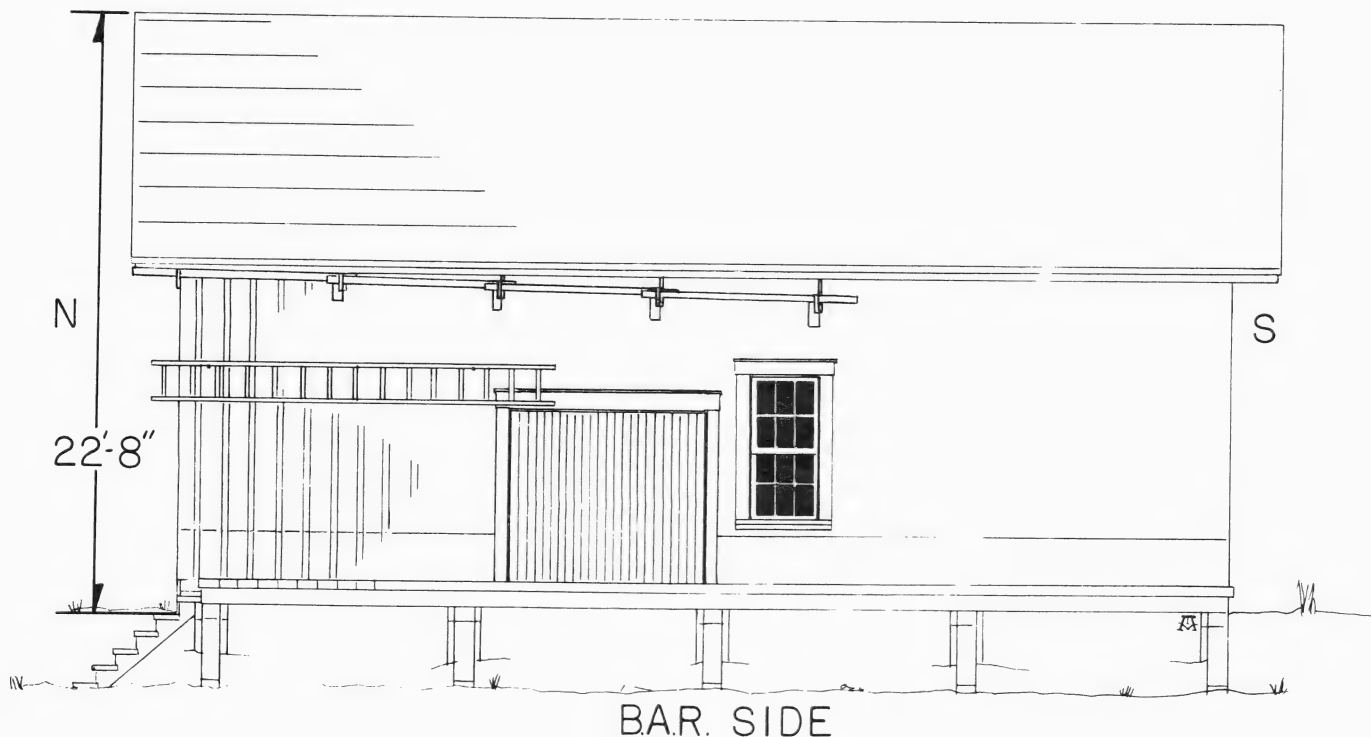
By 1938 the passenger traffic had dwindled to the point where repairing the one combination car was too expensive. This car was simply "bad ordered" and shoved onto a siding at Monson. Regular

passenger traffic was curtailed and occasional passengers were accommodated on the home-built track-mobile. Some items of express and mail were handled in the company stake-bodied truck.

In its last years the Monson's management did little repair to trackwork or equipment. Soon the previous occasional derailments became every trip events. In 1943, with the lessened demand for slate, the Monson Slate Company filed for bankruptcy. A scrapper was found who hauled all the equipment to the junction and burned it for the metal parts. The rail was pulled up in 1944 and the Monson RR was dead. Linwood Moody in *The Maine Two*



Drawn by Al Tenney



Footers (Howell-North, 1959) describes the Monson as ante-bellum and antediluvian due to its continued use of stub switches and link and pin couplers. I can't think of any better way to sum up the Monson than these words.

Now let's turn our attention to the layout of the junction and the map. "A" on the map locates the freight shed near the depot. The freight house that existed at the end of operations is shown in the drawings and photos. This last freight house appears to have been rebuilt from a portion of the original structure sometime between 1915 and 1920. The original structure was 113 feet long, 28 feet, 2

inches wide and 22 feet, 8 inches tall at the peak. The original shed had board and batten vertical siding and was unpainted. The roof was slate covered, probably standard on Monson RR buildings.

The later freight shed had the same height and width as the original, but the length was only 39 feet, 8 inches. It also had vertical board and batten siding. The upper portion was painted buff. A band along the bottom, under the windows was painted with what looked like a mixture of black and green. Tar paper covered the roof.

"B" on the map locates the set of seven connected squares on the other side of the

BAR tracks. After some cogitation on their meaning and numerous letters and telephone calls to New England I still am not sure what they were. Two possibilities seem plausible. Stock could have come from the farms around Abbott Village, and these could have been stock pens. It seems more likely that they were coal bins, however, since areas five, six, and seven were shown covered with a roof in an earlier map.

For the modeler, the layout of the junction offers some mainline standard gauge modeling mixed with narrow gauge. The narrow gauge section offers some interesting switching problems and the

freight shed offers a reason for operation. The "true to prototype" modeler will note that the entire junction can be modeled without truncating distances.

Well, I'm gonna pull the pin on this column. In closing, I want to thank Al Tenney of Vermont who has made a personal effort to fully document the Monson RR. He was most helpful to me in the preparation of this column and provided the freight house drawings and data.

Narrowly,

Bill Jensen
Coon Valley, WI 54623



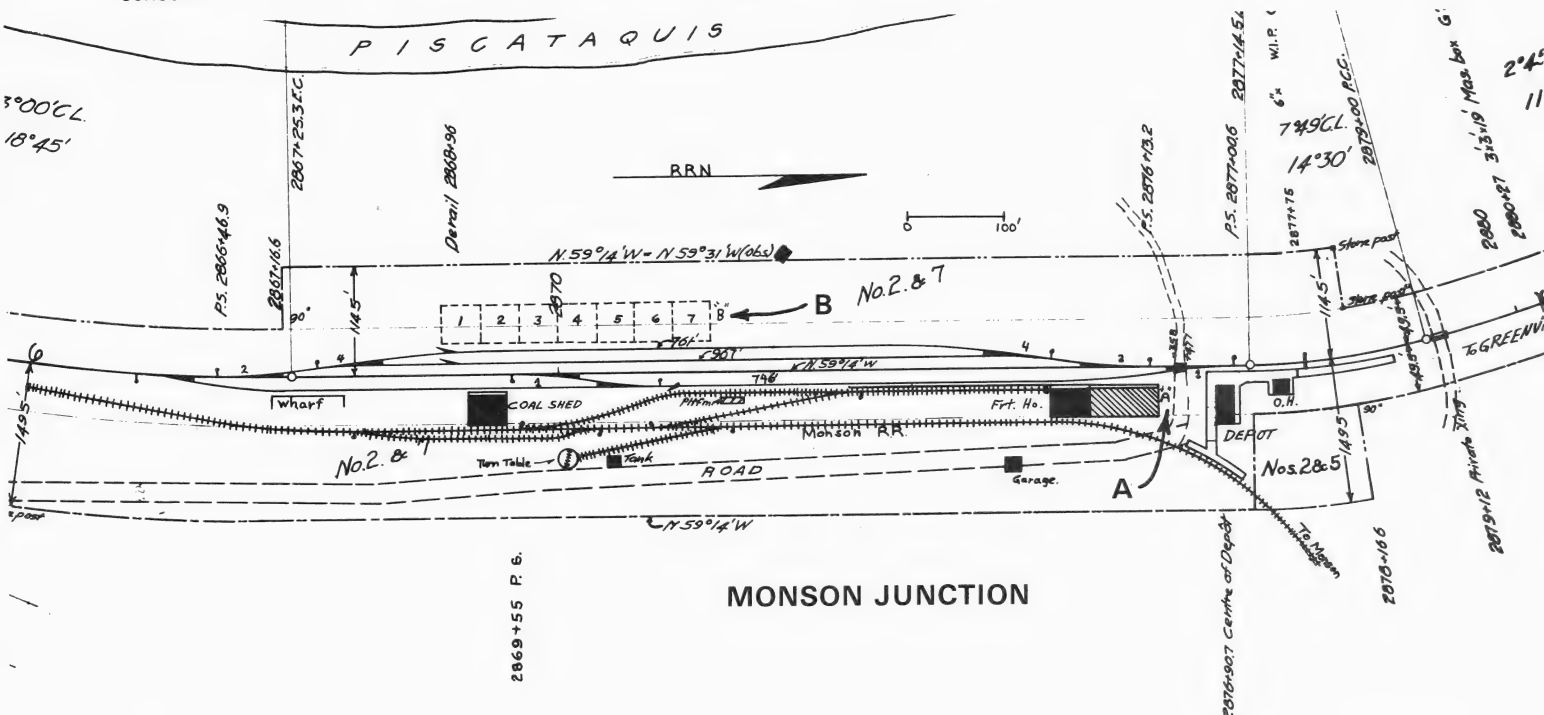
Engine #4 entering the junction to complete some switching. The freight shed is partially hidden by the locomotive. Photo by L. Moody, collection of Bill Jensen.

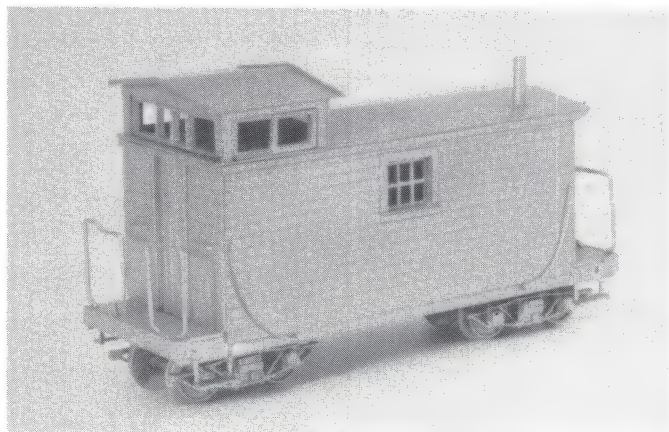


The Monson Junction depot as seen from the Bangor & Aroostook tracks. The outhouse is on the left of the photo. The freight shed is on the right. Photo by L. Moody, collection of Bill Jensen.



A flatcar of crated slate products is being unloaded into the freight shed. Photo by Blake, collection of Bill Jensen.



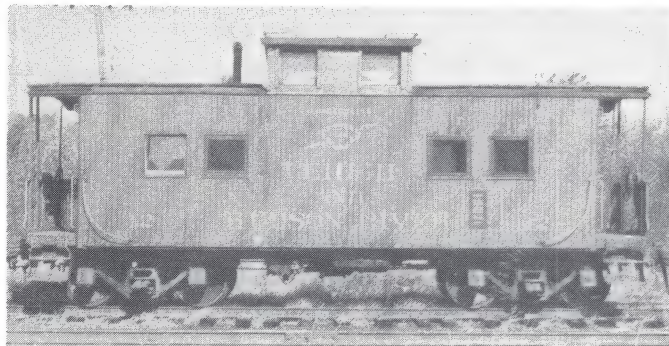


AT YOUR DEALER.....

The West Side Lumber Company Caboose in HO scale

This little Caboose is a limited run edition and comes complete with trucks.

Only \$37.00 at your dealer.

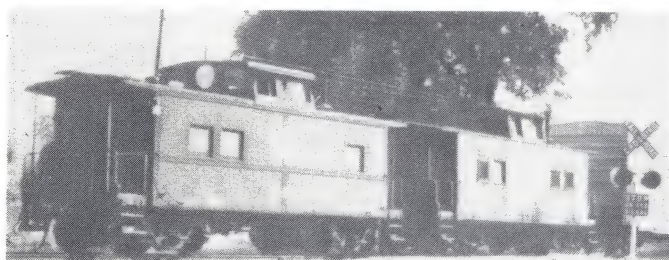


ANNOUNCING THE USRA CABOOSE

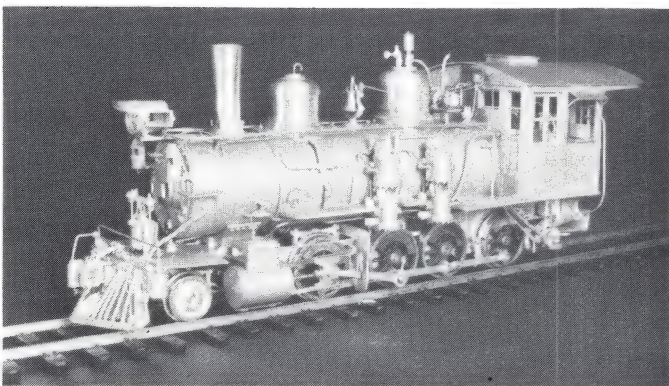
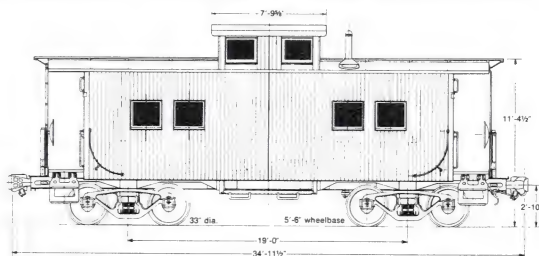
This "crummie" was used on many railroads in the east. The Lehigh Valley, B&O, Erie and a half dozen others made use of the United States Railway Administration's handy work.

Available in "O" scale and at your local dealer this September.

3 Versions: Two Steel & One Wood Sided.



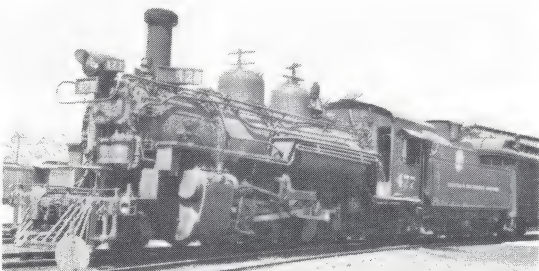
Prototype Photography Collection: RAILROAD MODEL CRAFTSMEN.



THE C-16 Number 268 in On3

PILOT MODEL

Sunset Models presents its newest locomotive in grand form, will have embellished in it all of the refinements of the K-28 that was imported earlier this year. Complete with a fully detailed cab by Precision Scale Company, refined drive system and a 22MM Canon can motor. This will be a very limited run!



NEWS ON THE Sn3 K-28

Production is planned for D&RGW K-28 for 1979. Be sure to pre-order from your local dealer.

Prototype Photography by H.K. Volrath.



SUNSET MODELS

19986 MALLORY COURT
SARATOGA, CA. 95070

WMA

·CORONADO· SCALE MODELS

YOUR ONE STOP SOURCE FOR NARROW GAUGE SUPPLIES!

GRANDT LINE PRODUCTS

3514	O Scale Picket Fence	1.25
3515	O Scale 24" dia. Cable Sheave 2 for 1.00	
3610	O Scale Shed Door 4 for 1.00	
3029	On3 Bettendorf Truck	6.75
5119	HO Picket Fence Set	1.25
5120	HO On3 Bettendorf Truck	4.50
5121	HO On3 Pass. & Tender Brake Set	1.00
5122	HO 43" dia. Cable Sheave 2 for 1.00	
3030	On3 Seated Figure with Derby Hat	2.00
3030P	On3 Painted Figure	5.00
3031	On3 Seated Figure with Felt Hat	2.00
3031P	On3 Painted Figure	5.00
On3	Drivers for Kemtron 0-6-0T	40.00
On3	Drivers for Kemtron 2-8-0	53.00
On3	Micro-Mo Drive for above	65.00

THOMAS A. YORKE ENT.

O-106	O Small Brick Store	32.95
O-107	O Stone & Wood Store	34.95
O-109	O 2-Story Brick Store	44.95
O-110	O Comp'ny Housin'	24.95
O-110-3	3 pack of Comp'ny Housin'	70.00
O-111	O Fire Hall	27.95
O-112	O Blacksmith Shop	coming

PRECISION SCALE CO.

HO On3	West Side Lbr. Shay No. 8 KIT	190.00
--------	-------------------------------	--------

NORTHWEST SHORT LINE

O	Minaret 2-8-2T	650.00
---	----------------	--------

EVERGREEN SCALE MODELS

801	Halls Hardware kit	24.95
-----	--------------------	-------

CLASSIC MINATURES

CM-29	HO On3 SP Keeler Engine House	9.75
CM-20	HO On3 RGS Ophir Depot Kit	21.00
	HO On3 Durango Depot Plans	2.00

THE WHEEL WORKS

WW-109	HO FORD Small Stake Truck	5.95
--------	---------------------------	------

CHOOCH ENT. (1/4" Scale)

5109	Coca-Cola Machine	1.98
108	Pass. Brake Beams & Shoes	6.95
109	Freight Brake Beams & Shoes	6.95
111	1890 Wood Beam Tk. less/wh	22.95
112	1930 Bettendorf Tk. less/wh	19.95
113	Bettendorf "T" Tk. less/wh	15.95
114	Diamond Archbar Tk. less/wh	18.95
115	Barber Archbar Tk. less/wh	16.95
118	Leaf Spring Diamond Archbar	19.95

RIO GRANDE MODELS

HO On3	D&RGW Pile Driver OB	36.95
HO On3	D&RGW Idler Flat for OB	7.50

C. H. B. MODELS

MT-510	O Scale Vertical Drill	16.95
MT-500	O Scale Engine Lathe	16.95

IRON HORSE MODELS

HO On3	WP&Y ALCO Diesel	190.00
--------	------------------	--------

WESTSIDE MODELS

HO On3	D&RGW Fishbelly Flatcar	29.95
HO	SP MK-10, 2-8-2	145.00

SANDY RIVER CAR SHOPS

HO On2	SR&RL Box Car #85	7.50
HO On2	B&SR Box Car #56	7.50
HO On2	Edaville Box Car	7.50
On2	B&SR Flat Car #8	10.95
On2	Ties (200)	1.75
On2	Switch Ties (150)	1.75

DURANGO PRESS

DP-64	On3 RGS Rotary No. 2 Kit	200.00
DP-58	HO On3 NM Lbr. Log Car Kit	5.95

THE TRAIN WORKS

On3	Finescale Standards Gauge	2.95
-----	---------------------------	------

E&B VALLEY RR CO.

HO On3	RGS Caboose #0404 Kit	14.95
--------	-----------------------	-------

FAST MAIL ORDER SERVICE OUR SPECIALITY!!

PP 3rd CLASS or UPS - 1st CLASS 10%

PHONE 602-254-9656



·CORONADO· SCALE MODELS

1544 E. Cypress Street

Phoenix, Arizona 85006

Announcing an Exciting NEW Calendar for 1979 HAWAII AND ITS RAILROADS



1879-1979



\$5.95

From the author of "Sugar Trains," "Narrow Gauge in a Kingdom" and "The Sugar Trains Pictorial" comes the 1979 Hawaiian Railroad Centennial Calendar. Jay C. Conde celebrates One Hundred Years of rail operations in the Fifth State of the Union with twelve full color paintings by artist Mike Kotowski featured in this superb, once in a lifetime collector's item.

Available around August 1, 1978 at your local bookstore, hobby shop or direct.

When ordering direct send check or money order to:
J. C. Conde • P.O. Box 146
St. Matthew Station
San Mateo, Calif. 94401

Calif. Res. add 6% sales tax.

VIEM

National Motor Co.

OCT 15

1929 PACKARDS
12 MODELS TO CHOOSE FROM

P.O. BOX 931
WALLA WALLA,
WA. 99362



- PHAETON SPEEDSTER
- ROADSTER COUPE
- SPORT PHAETON
- BOATTAIL SPEEDSTER
- VICTORIA CONVERTIBLE
- STANDARD EIGHT SEDAN
- CLUB SEDAN

* SPECIFY TOP UP OR DOWN ON FIRST FIVE MODELS.

NOTICE

WE HAVE INVESTED MANY THOUSANDS OF DOLLARS TO BRING YOU THESE FINE CARS. TO HELP RECOVER SOME OF THIS INVESTMENT AND TO HELP KEEP PRICES DOWN, WE HAVE DECIDED TO OFFER THEM ONE TIME - AT SPECIAL PRE-RELEASE PRICES AS FOLLOWS -

3 FOR \$12.00 7 FOR \$25.00 ALL 12 FOR \$42.00

WASHINGTON RES. ADD 5% SALES TAX. CANADIANS ADD 10% - SORRY!

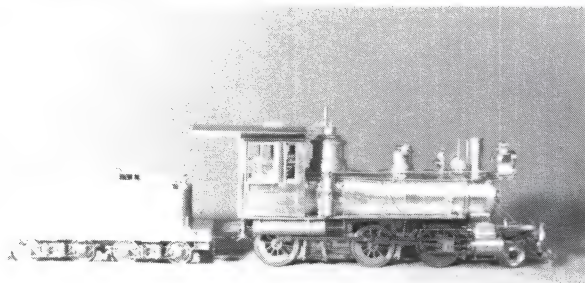
~ FEATURING ~

- DLX. WHITE METAL KIT
- EASY PAINTING
- FAST-EASY ASSEMBLY
- SUPER DETAILED
- NO FRILLS PKG, YOU GET THE SAVINGS.

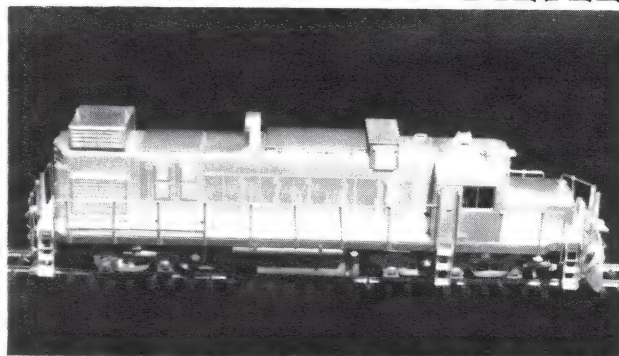


Iron Horse Models

"NOW AVAILABLE" IN HO_n3...THE WHITE
NCNG No. 5 2-6-0 in On3 PASS AND YUKON DIESEL



A very rare locomotive for an admirer of beautiful craftsmanship and custom assembled brass engines, the Nevada County Narrow Gauge Number 5 is now available. The first shipment is at your Iron Horse Models dealer. The second shipment (38 units) is due soon. TOTAL RUN - 98 locomotives!



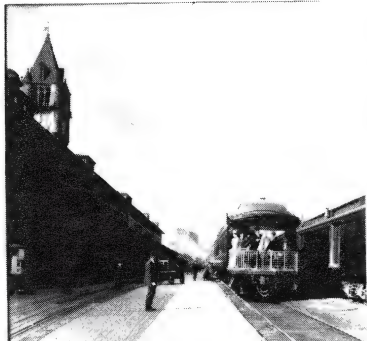
Don't miss her, she's a beauty AT YOUR DEALER. First 180 units in. Second shipment delayed because of the devaluation of the dollar.

Box 1802
Davis, Calif. 95616



VANISHING VARNISH

By GREGORY LE PAK



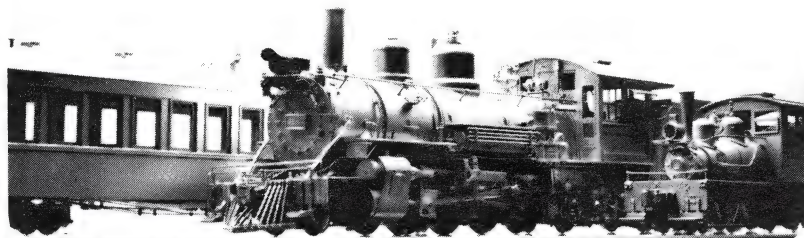
This epic 9X12", 256 page book recreates the Rio Grande's passenger service from 1871-1978. Rare pictures include D&SL & RGS trains plus several early narrow gauge depots. Photos are published for the first time with several in full color.

- Edition limited to 2000 signed and numbered hardbound books
- Numerous passenger coach drawings with interiors accurately illustrated
- A review of the Rio Grande's private cars
- Dealer inquiries invited

Alpine Publishing Ltd.
5031 West Fremont Drive

Publication September '78 Send \$26.95 to: Littleton, Colorado 80123

Colorado Standard Gauge



DOWNTOWN HOBBY

Is really getting to be a thing with us at Downtown Hobby. We're building such a complete collection of D&RGW, C&S and other narrow gauge brass and kits in all scales that we thought we would just title this ad appropriately to prove we know the "lingo" and let you know we certainly want to try to meet your narrow gauge needs.

22 WEST MADISON ST. • SUBWAY ARCADE
CHICAGO, ILLINOIS 60602 • 312-372-2464

NARROW GAUGE & SHORTLINE

GAZETTE

Back issues available:

MARCH, 1977
MAY, 1977
SEPTEMBER, 1977
NOVEMBER, 1977
JANUARY/FEBRUARY 1978
MAY/JUNE 1978
JULY/AUGUST 1978

All back issues are \$2.25 and are sent 2nd class postpaid. Please allow three to four weeks for delivery.

Gazette Publications
P.O. Box 26
Los Altos, CA 94022

Franktown Water Tank

First of a series of
Virginia & Truckee
cars and structures

O-SCALE \$21.00

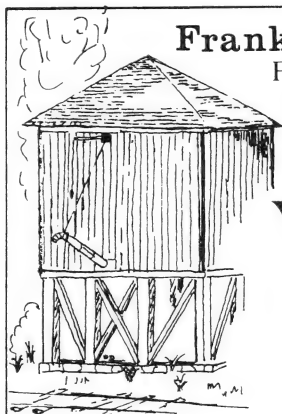
HO \$12.95

Also used on the
Carson & Colorado
at
Candelaria

V. & T. SHOPS
P.O. BOX 5597 RENO, NV. 89513

Large SSAE for list

Direct or at your Dealer



THE

TRAIN WORKS

TRAINS
O-HO-N

SCALE
MODELS

SCALE MODEL RAILROADING

WEEKDAYS 4-9 SATURDAY 10-6 SUNDAY 12-5 CLOSED WEDNESDAY

Try us for your model railroading needs, in both standard and narrow gauge. We carry a large inventory of brass models, kits, detail parts, scratchbuilders supplies - with emphasis on 1/4" scale. Tools, trackwork supplies, paint, sound systems, power packs and railroad books: all carefully calculated to whet your appetite and deplete your bank account.

We also offer a custom painting service - an SSAE will bring you our description and price list.

9525 Bolsa Ave. Westminster, Ca. 92683 (714) 531-8260



Courtesy of Robert Sloan

Book Reviews

Mineral Belt – Volume I
Sundance Publishing Co.
P.O. Box 597
Silverton, CO 81433

This long-awaited first volume of a series on Colorado's fabled mineral belt has been published by Sundance. Termed the most definitive work on the old South Park (later C&S), Volume I covers Denver to Leadville via the South Platte Canyon and the great South Park area of Colorado. The book is about 400 pages long, with numerous black and white and color photographs. In addition, there is a short summary of the South Park's corporate history, motive power, and rolling stock. Plans are also included for a caboose, two Mason-Bogies, a Cooke 2-6-0, passenger cars, and a DSP&P Tiffany refrigerator car.

It is a very good book, but you should not expect this volume to be "the most complete history of the South Park," as it has been advertised; it simply is not. Granted that Volume I covers only part of the DSP&P's system, but even that notion is not covered with the same depth and detail as found in two other books. More depth and detail are found in Mac Poor's *Denver South Park & Pacific* (Rocky Mountain Railroad Club, Denver, 1949) and the *Colorado Railroad Museum's Annual #12* (Golden, Colorado, 1975). Each location between Denver and Leadville is described but not in depth. Although Como was an important division point, it is only cursorily mentioned. And the Boreas Pass Route is not covered at all! (Perhaps it will be described in subsequent volumes.)

In trying to cover the geography, geology, history, and mining practices of a region, the book can only cover each topic lightly. Indeed to cover Colorado's mining history alone would require a fat volume.

Thus, one is left with the impression that the book tries to cover too much too quickly. Granted the information is valuable and the juxtaposition of topics puts each element into a context, but perhaps a little less ambitious approach would be better in the future.

There were also a number of minor irritants. First, the author spends a fair amount of time "promoting" the other proposed volumes in the first few chapters. This might be more appropriate for advertisement. Secondly, the author editorializes far too much with comments disparaging today's "wasteful" society (i.e., energy wasteful, etc.). No matter how sincere the author may be in his views, a work of history is just not the place for editorials or current political problems. Such comments become quickly outdated.

A great amount of the relatively high cost of this work is due to a multitude of color photographs. Color adds much to a book, and this one is no exception. A most dramatic and fascinating feature is the juxtaposition of a historic sepia tone view and modern color view, shot from the same angle. Less helpful are postcard-style views of modern Denver and Red Rocks Stadium, as well as some general color scenery shots whose value is questionable.

A large amount of space is devoted to early views of Leadville and Denver, including a history of Denver's Union Station. Many of these pictures (especially of Denver) have appeared in earlier Sundance works such as "Colorado on Glass." The Leadville section is especially good with many rare mining shots, and is quite informative.

Despite the critical concerns expressed above, I was generally impressed by this book. Obviously the author has invested a lot of time and effort into it. It includes information and photographs available nowhere else. Its regional approach to history is novel for a "rail" book and shows a lot of imagination on the part of the author. And it is a readable, entertaining work. I purchased a copy because it does present good information and I look forward to Volume II. I recommend it to any Colorado and Southern fan, or even the reader interested in the history of the Leadville mining district.

Mineral Belt, Volume I retails at \$44.95.
Charlie Getz.

HO Cut and Assemble Early New England Village

by Edmond V. Gillon, Jr.
Dover Publications, Inc.
180 Varick Street
New York, NY 10014

Don't turn up your nose at this one simply because it is a "cut and assemble" book. First of all, it is an extremely well produced work with excellent color repro-

ductions of 12 authentic HO-scale models of typical New England architecture and easy-to-follow instructions. Second, the author has thoughtfully added many extras such as the history of each structure and attendant details like fences and a bandstand. For those of you enamored of Maine two-foot railroads, this book is a natural — a week of evenings with a No. 11 X-acto Blade and some white glue should result in a complete village. You might also wish to add strip wood strengtheners to prevent warpage. The buildings are also usable as background structures, as "fill-ins" to be later replaced by scratch-built duplicates, or the fronts can be pasted onto a backdrop for 3-D effects. More ideas will occur to you, I'm sure. We were really impressed by the quality, authenticity, and results possible with this book. At \$2.50, it is a bargain beyond compare. *Charlie Getz.*

Clover House Catalog

Clover House
P.O. Box 62,
Sebastopol, CA 95472

Clover House has their catalog #2 available. It lists all of their products and includes window glass and scribes, HO, S, and O scale lumber, ties in HO, S, O, and N scales, poles, chain, and wooden barrels. This useful catalog is well worth sending for. Send 75¢ to the address above. *Bob Brown.*

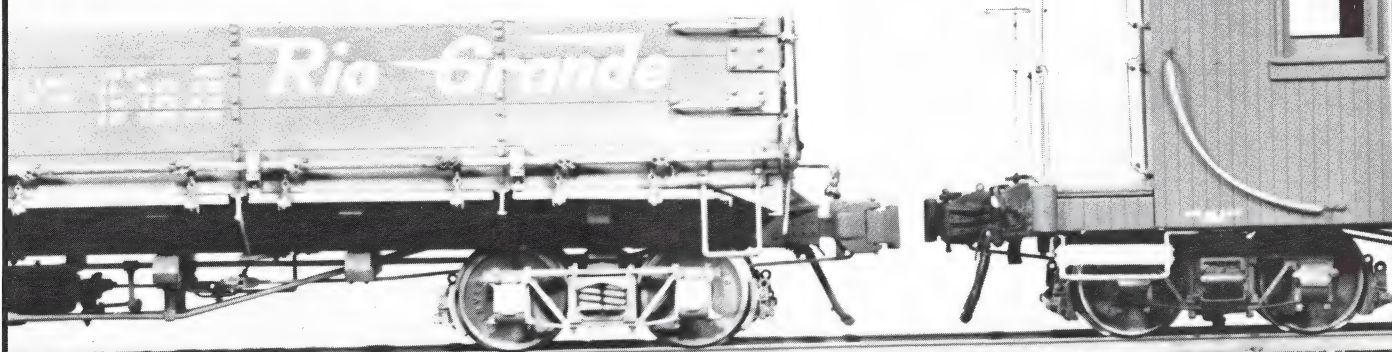
More Railroads You Can Model

Edited by Mike Schafer
Kalmbach Books
1027 North Seventh St.
Milwaukee, WI 53233

This 78 page paper bound 8½ x 11 inch book describes nine railroads worth modeling. Several will be of interest to **GAZETTE** readers. Each of these railroads is described in detail and a track plan is provided. Each has also appeared in the **MODEL RAILROADER**. Chapter one discusses some techniques you can use in adapting your favorite prototype into a model railroad. There is also an annotated bibliography listing additional sources of information for the railroads mentioned in this book.

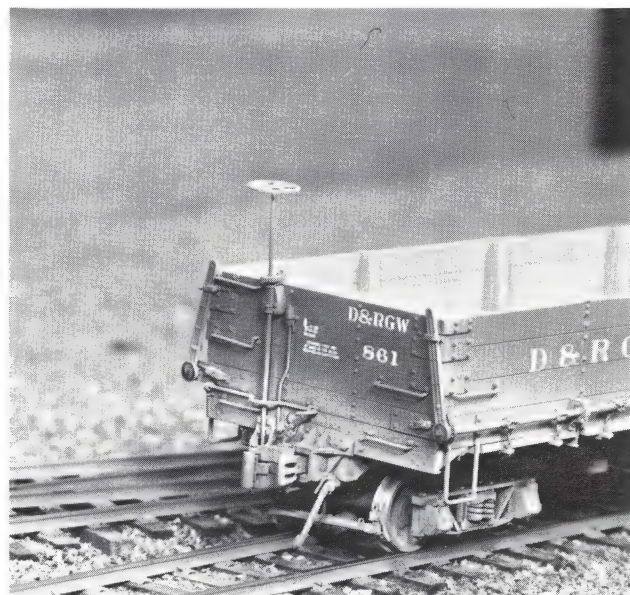
The railroads included in this book are: the Monon, the Graham County Railroad, the Boston & Maine's Gloucester branch, the McCloud River Railroad, the PRR's Horseshoe Curve, the Milwaukee Road's Beer Line, the White Pass & Yukon, the Maryland and Pennsylvania, and the Chicago Aurora & Elgin. The books is fully illustrated and graphically excellent. Look for it at your dealers or order direct. The price is \$4.50. *Bob Brown.*

AT LAST!



a SCALE 0 gauge OPERATING COUPLER

Pacific Fast Mail is pleased to announce the AutoScale® coupler* for O and On3 models. This coupler is completely scale in size and appearance and will afford the fine scale modelers an opportunity to enjoy "hands-off" coupling & uncoupling without sacrificing the scale appearance of a finely detailed model.



OPERATING FEATURES OF THE PFM AUTO SCALE® COUPLER:

- Couples with very low coupling force ... even your lightest cars will couple at crawl speeds.
- Uncouples remotely by means of electromagnets placed under the roadbed.
- Uncoupling magnet poles protrude only to the top of the ties for easy concealment.
- Automatic coupling and uncoupling will occur even though one car has a dummy coupler!
- Magnetic actuating lever is concealed under the car body to enhance realism.
- Standard size square shank facilitates installation on all cars.
- Two and three-tang short shank versions available in the near future.

*Patents pending



Pacific Fast Mail, P.O. Box 57, Edmonds, WA 98020.

In Canada: **VH Models**, 5816 Cambie, Vancouver, B.C.

In Europe: **Fulgurex SA**, 33 Avenue de Rumine, Lausanne, Suisse.

In Great Britain: **Micro Metalsmiths Ltd.**, Kirkbymoorside, York YO6 6DW, England

Celebrating 25 years of leadership in imported brass models — 1953 - 1978.

ELECTRONIC POWER CABTROLLERS



THE FINEST MODEL RAILROAD ELECTRONIC THROTTLES AVAILABLE
Our customers THE ONLY UNIT THAT THE OTHERS
must agree. ARE MEASURED BY

FANTASTIC BEAUTIFUL CONTROL - FANTASTIC FOR
OPERATION - Beautiful!!
Best Pack I ever operated for
It works like the REAL THING
I challenge it so much
I am still amazed by the great performance
GREAT! NEW CONCEPT TO THE OPERATION



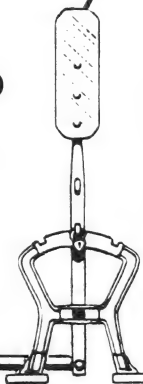
QUALITY POWER SYSTEMS INC
REALISM 56 BELLIS CIRCLE
POWER CAMBRIDGE
FROM PSI MASS
02140

Send Stamped, Self Addressed business Envelope for FREE Catalog

Ron's Car Shop

Custom ON³ & O
Car &
Locomotive Kit
Construction

1334 Vinca Way
San Diego, CA 92114



Are you operating 1/4" scale equip-
ment on HO 3.5mm rails? Why not
go 1/4" scale all the way and use the
only accurately scaled rail in O
gauge. Our 75# ASCE section is
ideal for narrow gauge and light
standard gauge lines.

HEIGHT .100
HEAD .051
BASE .100

Can be used successfully where 70-
80# rail is required. In nickel silver
only, 99¢, \$20.00 Postpaid. SSAE
for list



308 N. Mansfield Avenue
Los Angeles, California 90036

BOOKS

RAINBOW ROUTE; Sloan & Skowronski - with authors inscription \$35.00
Detailed History of SRR, SG&N & SN, 415 photos, 19 O scale plans, maps, 416 pp.
SILVERTON RAILROAD CO'S. - Sloan \$3.75
Short history SRR • SG&N • SN, 13 HO plans, maps, 52 pp.
STRUCTURES of the OLD WEST - Crea & Bell \$4.95
20 HO plans, Mills, Mines, Depots, Tank, Coal Ramps, Stores etc. 60 pp.

BRASS PHOTO ENGRAVINGS

	HO	S	O
2 truck RGS Galloping Goose	\$15	\$20	\$30
3 truck RGS Galloping Goose (both bodies)	\$15	\$20	\$30
Silverton Northern "Casey Jones" railbus	\$10	\$20	\$20
2 foot gauge Hinkley or Portland O • 4 • 4t	\$20		\$40
2 foot gauge Baldwin 2 • 4 • 4 • T B&SR6, SRRL8	\$20		\$40
C&S 57-62, 63-70, 4-10 cabs & tenders cabs	\$5 each	\$10 ea	
D&RG 346, 318, T-12, C-21, RGS cabs & tenders	\$15	\$30	Per road set
SN 3, 4, 34 cabs & tenders, Colo., Mid. cabs & tenders	\$15	\$30	of 3 or 4

DECALS

	N	HO	S	O
Westside Lbr. Co. Locos & cars		\$2		\$4
Colorado Work Cars (white)		\$2		\$4
Pre-1906 Colo. Locos (mixed gold & white)	\$2	\$4	\$4	\$4
D&RG - Mears roads freight cars 1870-1921	\$2	\$2	\$4	\$4
South Park - C&S freight cars 1872-1942	\$2	\$2	\$4	\$4
Two 1883 D&RG Billboard Reefers	\$2	\$2	\$2	\$2
Fancy Gold Passenger Cars all NG	\$1	\$4	\$8	
Catalog & Lettering Guide	\$1			

ROBERT SLOAN 30 E. Pleasant Lake Road,
St. Paul, MN 55110

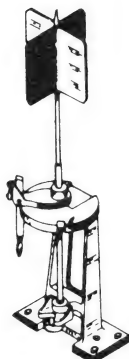


ATTN. PROSPECTIVE HOBBY SHOP OWNERS

If you are planning on opening a new hobby shop think about using the
best scale lumber available today. **KAPPLER MILL & LUMBER COMPANY**
provides the finest in N, HO & O gauge scale lumber and wood accessories
such as scribed wood and 26' pre-built ladders. Send a S.A.S.E. for price
sheets and dealer information.

MODELERS please send a S.A.S.E. for **KAPPLER'S** list and ordering info.
Evergreen Hill Designs • P.O. Box 396
Belmont, Cal. 94002

'REDUCED PRICES' - SALE!



I.H.M. D&RGW "6500" Steel Flat On3 \$88.50
I.H.M. D&RGW Texaco Two Dome Tank On3 109.95
I.H.M. Baldwin 2-8-0 Ma & Pa 'O' 799.95

W.S. CONX Tank Car On3 69.95
W.S. F&CC Caboose On3 46.95
W.S. D&RGW Derrick & Flat On3 87.50
C.B. C&S Caboose On3 46.95
C.B. Sandy River Caboose On2 49.95
C.B. Sandy River Coach On2 109.95

THE BACK SHOP Box 2510-N, Huntington Beach, CA 92647

PERIOD MINIATURES
IRON HORSE MINIATURES
16 Ton Shay On3
Baldwin 4-4-0 On3
MA & PA 2-8-0 O
NORTHWEST SHORTLINE
2-6-0 On3

\$715.00
\$762.00
\$955.00
\$219.00



We stock complete lines of Precision, Kemtron, and Calscale detail parts in O, On3, HO and HOOn3.

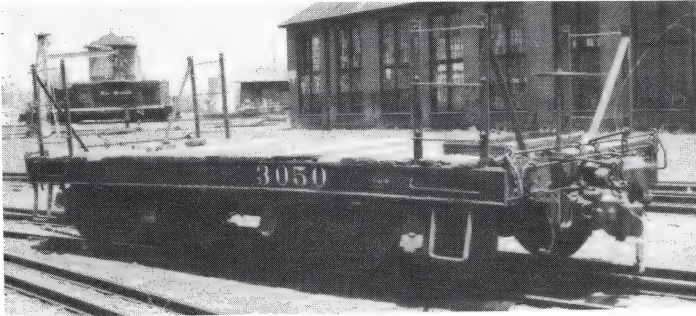
Roy's Train World

1033 S. Country Club Dr.
Mesa, Arizona 85202
1-602-833-4353

DURANGO PRESS On3
Westside Snow Plow
KEMTRON On3
Class B Shay Kit
C-16 Modern Kit
C-16 Original Kit

\$44.95
\$240.00
\$225.00
\$225.00

Mail orders—Cash, check or Master Charge or Bank Americard. Number and expiration date required on Bank Cards.



READY NOW THE LONG AWAITED D&RGW THREE WAY COUPLER CAR.

Just the piece of equipment for your dual gauge pike. This fully operational car is designed to use Kadee HOOn3 couplers. This easy to assemble kit features highly detailed white metal castings, scale wood by Kappler, decals by Thinfilm and detailed drawings by Ed Hauff. Comes with trucks featuring non-magnetic axles and blackened wheels.

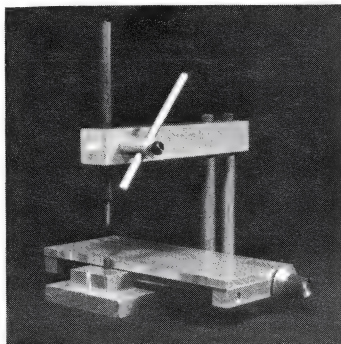
**RIO GRANDE
MODELS LTD.**

Box 4463 Santa Clara,
California 95054

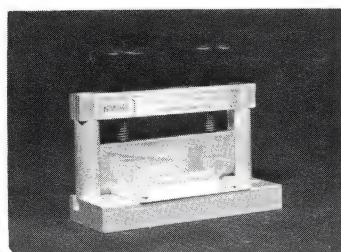
HO scale \$11.95
Cat #3026-CC

available at your
dealer or direct

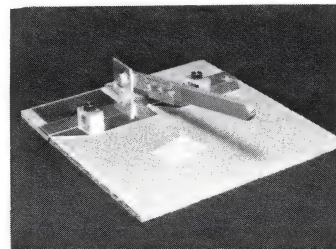
TOOLS FOR THE MODEL BUILDER



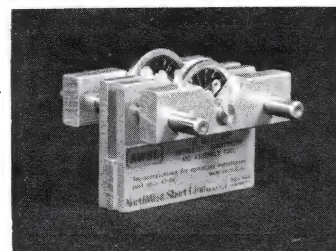
THE SENSIPRESS+
A sensitive arbour press small enough to give you that fine touch for working with fine models and parts. The 3/8" ram accepts interchangeable tips (also available) to fit your needs. \$39.95. Also shown here is **THE RIVETER+**, a calibrated advancing table which converts the Sensipress into a precision rivet embosser. \$19.95



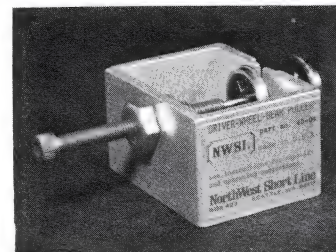
THE BENDER
A miniature press brake to bend brass or other light material up to 90° on work up to 3" wide. Works great on styrene too. \$19.95



THE CHOPPER
One of the most versatile and usefull tools you'll ever use. This heavy duty strip-wood cutter provides clean, neat, accurate cuts faster and easier than ever before. A must for bridge builders. \$9.95



THE QUARTERER
Accurate quartering is essential to getting smooth operation from your locomotives and this tool allows you fast, easy, and accurate quartering of drivers up to 1" OD on 1/8 or 3mm axels. \$29.95



THE PULLER
A tough, precision tool for removing wheels, drivers, and gears from axels and shafts. 1/16 and 1/32 press tip screws, allen wrench, and operating suggestions included. \$4.95

"O" SCALE! The Highlands Station



This beautiful diminutive structure still stands on the Burlington mainline in Hinsdale, Ill. The 1/4" scale is very close in detail to the prototype.

The Highlands Station Kit M 516 \$45.00

- All details cast in walls in durable polyester.
- 22 cast metal parts
- Shake-shingle roof included.
- Stripwood rafter ends and headers.
- Measures 6-5/8" x 3-7/8" at base.

KIT INCLUDES STRUCTURE ONLY

See your dealer. If he cannot supply you, order direct. Add \$1.00 for postage and handling. Illinois residents add 5% sales tax.

Magnuson Models, Inc. P.O. Box 199, Lake Villa, Illinois 60046

RAILROAD BOOKS

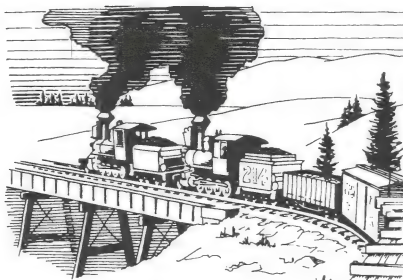
and Western History

Hundreds of titles in stock — send for our catalog. We also publish the popular **COLORADO RAIL ANNUAL** series:

No. 8 — Scenic Line of the World original D&RG Denver—Black Canyon—Salt Lake 3' gauge \$9.50 ppd.

No. 9 — Galloping Goose — also Rio Grande "Prospector", Fred Jukes, C&TS RR (4 Stories) \$9.75 ppd.

No. 10 — Narrow Gauge to Central and Silver Plume — historic Colorado Central line over the famed Georgetown Loop \$9.75 ppd.



No. 11 — D&RG to Ouray — Otto Perry—Chihuahua ng \$9.75 ppd.

No. 12 — South Park Line — spectacular narrow gauge to Leadville; Gunnison via Alpine Pass \$14.75.

No. 13 — Florence & Cripple Creek Gold Belt — Phantom Canyon narrow gauge shortline — \$14.75 ppd.

from the Colorado Railroad Museum

DEPT. S, BOX 10,
GOLDEN COLO. 80401

ANNOUNCING The start of a line of SUPER-DETAILED FIGURES in Quarter Inch Scale



3030 3031
Unpainted - \$2.00
Deluxe Painted - \$5.00
Cast in White Metal from
Hand Carved Masters
by PETER ADUAN

Send 25c and S S A E
for Complete List

Grandt Line
PRODUCTS, INC.

1040 B Shary Court, Concord, CA 94518



**CAMINO
HAS MOVED...**

to larger quarters in order to
serve you better.

CAMINO SCALE MODELS

P.O. Box 10666
Eugene, Oregon 97401

Send for our catalog of fine scale
wood components and kits—still
ONLY 50 cents

BROADMOOR HOBBIES

DIVISION OF JLE

GRAND OPENING SPECIALS:

NWSL	"MINARETS" 2-8-2T	\$695.00
CB	LITTLE RIVER 2-4-4-2	795.00
PFM	D&RGW S.G. CABOOSE	80.00

LARGE SSAE FOR LATEST BRASS AND KIT LISTING

PLEASE NOTE OUR NEW SHOWROOM ADDRESS

785 South Church Street, Hazleton, PA 18201

Sorry...

We will answer all of our mail
from as far back as November
1977... Our move to new facil-
ities has set our correspondence
back a bit...

M. F. Kotowski

Narrow Gauge is **THE STANDARD**

WMC

HO _n 3 D&RGW K-37	199
D&RGW K-27	
Slide Original	149
D&RGW K-27 sl.mod.	149
D&RGW K-27 comp.	149
D&RGW K-36	260
D&RGW C-25	149
D&RGW C-16	125
D&RGW Flat Car	27
D&RGW K-28	350
D&RGW short caboose	40

IHM

HO _n 3 WP&Y diesel	199
-------------------------------	-----

KEY

HO _n 3 D&RGW C-19	130
------------------------------	-----

CUSTOM BRASS

HO _n 3 D&RGW C-21 #360 c/p	170
Flanger	39
San Juan set # 2	100
Rico car	55

PFM D&RGW K-28	199
----------------	-----

For your convenience, we now accept

MASTER CHARGE & VISA

PFM

Omaha 0-6-0	125
D&RGW P-44	160
So. PS-4 4-6-2 1st run	180
NP S-4 4-6-0	125

WMC

SP T-1 4-6-0	120
PRR D-16sb 4-4-0	115
SP PR-1 2-6-2	125

CUSTOM BRASS

D&RGW M-78	185
------------	-----

SUNSET

SP S-8 0-6-0	110
N&W K-3 4-8-2	130
GN F-1 2-8-0	115
B&O 1-5 cabooses	
7 or 8 windows	28
UP 4-6-0	125

KEY

HO ATSF #1000 & #1050	130
D&RGW M-64	199
D&RGW M-68	225
MM-D&RGW gallows turntable	22
MM-Durango Depot	30

IRON HORSE HOBBY SUPPLY CENTER

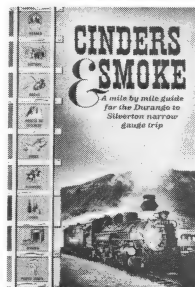
4544 W. JACKSON BLVD

HILLSDALE, ILL. 60162

(312) 544-7091



COMPANIONS FOR COLORADO NARROW GAUGE RAILROADING



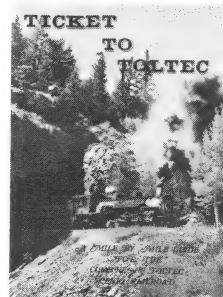
\$4.00 plus .50 post.

GUIDEBOOKS

by Doris B. Osterwald
with special sections on

EQUIPMENT • HISTORY
CONSTRUCTION • NATURE
OPERATION • GEOLOGY

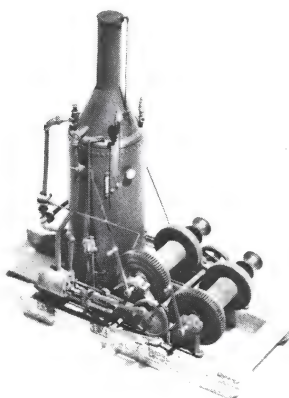
Both books postpaid \$8.00



\$3.25 plus .50 post.

WESTERN GUIDEWAYS BOX 15532, LAKEWOOD, CO, 80215

AMERICAN HOIST & DERRICK



The A.H.&D. Standard Model Donkey was one of the most popular machines in the heyday of the logging industry. Our HO scale model captures all the intricacy of the original.

HH503 \$14.00

Here's an HO scale A.H. & D. vertical boiler with many applications for your railroad or industries.

HH502 \$3.00



Hetch Hetchy Scale Models

P.O. Box 5174 Hamden, Conn. 06518

Rotary Snowplow Action on the Cumbres and Toltec Scenic Railway

April 21st through April 23rd, 1978.

Three days of snow fighting. 35mm duplicate slide sets are available of this strictly business steam rotary trip, the first since March 1962 under Rio Grande management. Rotary OY, 487 and 484 battled snow almost constantly for four days in order to open the line to permit the movement of equipment from Chama to near Antonito for the filming of a movie. It's all here—deep drifts, taking water in running board deep snow at Cumbres, a minor derailment, more snow, watering at ice encrusted Los Pinos Tank, all under blue skies. (An example can be found on the front inside cover of the August, 1978 **TRAINS**).

For a sample slide, detailed explanation sheet and price list, send \$1.00 (refundable with order) to:



AL CHIONE

PO Box 1655

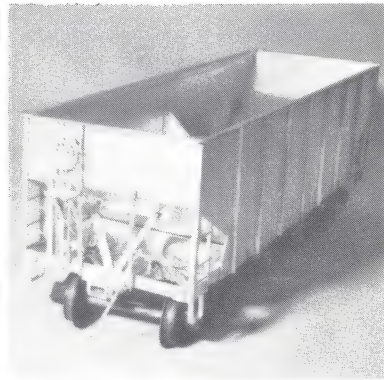
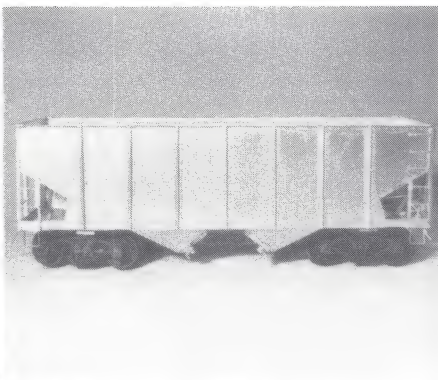
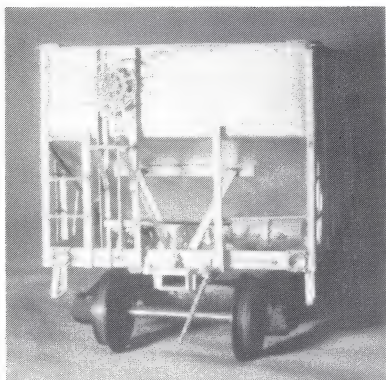
Durango, Colo. 81301



(916) 662-8823

DAVIS, CALIFORNIA 95616

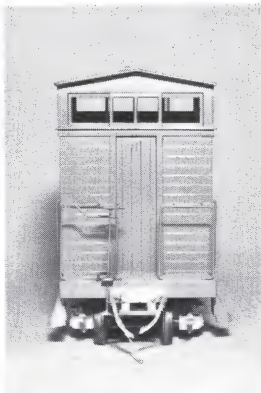
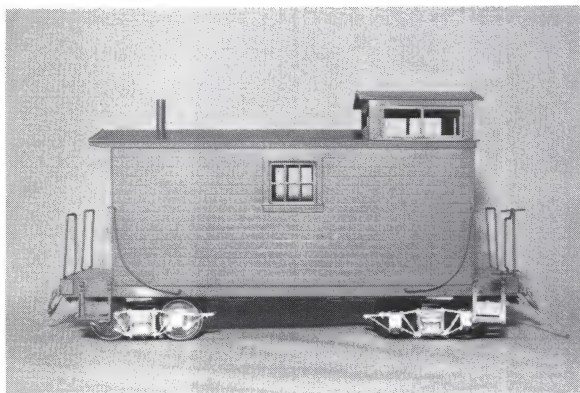
ANNOUNCING



Designed by Precision Scale Company, this 55 ton hopper car in "0" scale is the second model imported by PSC. All new patterns and castings are by PSC—designed for use with Kadee or PSC couplers.

"SEE OUR QUALITY"

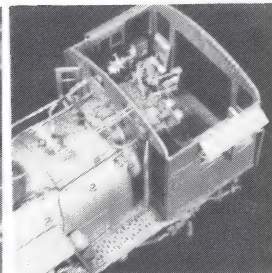
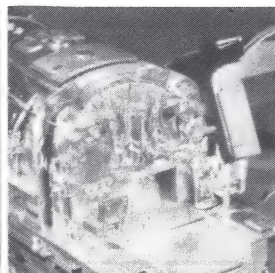
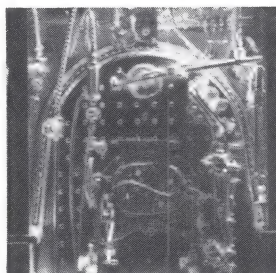
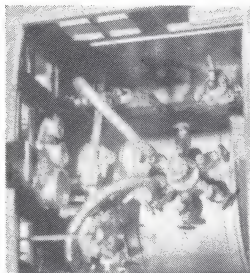
Due in September



All detail castings are by Precision Scale Company; superstructure and assembly by Lhee Do Industrial Company.

“AT YOUR DEALER”

Cab details & components



Send stamped self-addressed envelope (SSAE) for part number list PSHO-3700 series . . . cab interior detailing kits and other super structure detailing component parts for the D&RGW C-16, C-19, K-27, K-28, K-36 and K-37 locomotives.

Detailing kits for these same engines are available in "On3" scale. Some kits are complete with detail drawings* showing location of plumbing, gauges and other fittings.

Code 70 nickel silver rail on cinder brown nine foot ties; available in 3 foot lengths.

*Drawings by Kelly Morris.

RIP TRACK

\$1.50 for 15 words or less, name and address free. Additional words 8¢ each. Must be paid in advance. Closing date is the first of the month preceding the month of publication. For example, the January/February issue is published about the 15th of January and the closing date is the first of December. The other closing dates are Feb. 1 for Mar./Apr., Apr. 1 for May/Jun., Jun. 1 for July/Aug., Aug. 1 for Sept./Oct., Oct. 1 for Nov./Dec.

Please type or print your ads neatly. Thanks.

HIGH QUALITY custom painting and building at low prices. Locomotives painted, cars and structures in HO, O, and S scales built to your specifications. Specializing in commercial kits. All work done on bid basis. Send your specification information and SSAE for bid to Rico Grande Southern Shops c/o J.H. Hitzeman, 808 LePere, St. Louis, MO 63132.

commercial

NEW BRASS DISCOUNTED: Use this ad for a special price on a new Key HO D&RGW #1800 M-68 4-8-4 for \$229.99 prepaid — send cashiers check or use credit card. MC or Visa (add 3% for card) (send number and expiration date). Get our latest list in HO-HOn3-On3-O free. Send one or six SSAE to Douglas Models, 122 E. 2 So. St. Salt Lake City, UT 84111.

commercial

FOR SALE: All C/P WSM C-25 \$210.00. C-16 \$140.00. C/B 260 \$190.00. PFM Snd. Sys. \$295.00. Heath Trans. Pwr. Pk. assembled. \$85.00. Louis J. Jordak, 638 Ironwood Dr., Elk Grove Village, IL 60007.

GETTING STARTED in On2: We've got the track components you will need; code 70 #6 and #9 turnouts; code 83 #6 and #9 turnouts; ties; switch ties. We also have car kits and locomotive etching sets. SSAE for list. Ric's Etchings, 7805 S. Camrose Way, Sacramento, CA 95828.

commercial

FOR SALE: On3, unusual locomotives and cars (brass). Also unbuilt kits. Write for list. Gantlet, Box 95, Plainsboro, NJ 08628.

WANTED: Fine Scale Miniature kit #110 and other older FSM kits and the book "The Shay". Frank Markovich, 160 Northgate, Daly City, CA 94015.

CUSTOM BUILDING: painting, detailing HOn3, O, On3, & fine scale. Micro motor installations. Sound installations. Locomotive restoration. Satisfaction guaranteed. Sent SSAE for details. Eagle Locomotive Works, 2925 S.W. 19th, Gresham, OR 97030.

commercial

RAIL CARD: Full color 5½ x 8½ Giant post cards: send for catalog and price list. Dealers inquiries invited. Rail Card, P.O. Box 801, Manhattan, KS 66502.

commercial

OLD RAILROAD, state and county maps. 70-110 years old. All states. Stamp for catalog. Northern map Co. Dept. RT, Eagle River, WI 54521.

commercial

WANTED: Austin 4-2-4 "Huntington", Balboa 3 trk passenger "Goose", Kemtron Thomas rail car, Toonerville trolley, HOn3 dockside, MEW Mack ACX railbus, CUT 2+C+C+2 electric, Model Tramway Systems HO trolley sweeper, Huntington Model Works HO trolley sprinkler, Varney Aero Train. Bill Reckert, Box 605, Cucamonga, CA 91730. (714) 987-1314.

RAILROAD BOOKS at reasonable prices. Most current publishers. SSAE for list of available books. Railroad Equipment Dept. NG&SL, P.O. Box 231, Lindsay, CA 93247.

commercial

CUSTOM PAINTING AND REPAIR: Finishing to your specs. We build cars too — send a pic for accurate paint & weathering reproduction. Narrow gauge is my specialty. Send SSAE to Columbia Car & Foundry, Rt. 2 Box 802, Rainier, OR 97048. (503) 556-9249.

commercial

CRAFTSMAN SERVICES & SUPPLIES: Top quality custom finishing, detailing, motor conversions, sound installations and other special services at reasonable prices. Selected craftsman kits and supplies at considerable savings. Operated by experienced modelers to serve modelers needs. Send long SSAE for details. Curfewood Valley Railroad Shop, 20 Cohasset Drive, #G-2, Rochester, NY 14618.

commercial

AKANE: B&O EM-1 2-8-8-4, used, good, \$350 - USRA Heavy 4-8-2, used, good, custom painted, no lettering, \$250 - Balboa: SPGS-1, TRO, \$220 - Custom Brass: D&RGW C-21, 2-8-0, #360 or #361. \$160 - Key: D&RGW C-18, \$130 - Kemtron: C-16 intermediate or modern kit, \$100 - Mogul, \$110 - LMB: NYC Mohawk, TRO, \$220 - Niagra, TRO, \$260 - NPP: NYO&W 4-8-2 \$170 - Sunset: GN 0-4 2-8-2, \$150 - W&LE 4-8-2, \$135 - WSM: WSL Co. #8 HOn3 Shay, \$320 - C-1 3 truck HO Shay, \$320 - Sierra 2-8-0, used, remotored with Sagami, \$125 - GN A-9-0-6-0 dual compressors, \$150 - GN S-2, \$185 - SP A-6 4-4-2 F/P Daylight, \$190 - D&RGW C-16 #268, \$130 - M.O. - P.P.D. GTD - THE GREEN CABOOSE, 7 Sun Valley Dr., Medway, MA 02053. (617) 533-6272 Evenings & Weekends.

commercial

FOR SALE - HOn3 & HO brass (All D&RGW) TRO, all mint 100 series Timberline Kits, Tomalco, LaBelle, Sierra Southern, Rail Line, many other kits all unassembled. Lambert track and switches, everything priced to sell. Send SSAE for list: Tom Rush, 6589 Middle Urbana Rd., Springfield, OH 45502.

FOR SALE: U.S. Hobbies 13 Ton Shay, mint condition, original box, \$275.00. Bill Reynolds, 1915 Carlson Blvd., Richmond, CA 94804.

FOR SALE: On3 D&RGW #50 Diesel Switcher, unpainted, excellent condition, test run only. (303) 667-8248, Gene Howerton, P.O. Box 554, Loveland, CO 80537.

HON3 — Liquidating RGS and D&RGW locomotives, passenger trains, other rolling stock. Unopened kits by RGM, DP and others. Miscellaneous parts, scenery and track items. Electronic parts for SST-7's; also superbuttons, miniature switches. Books, SGN and **GAZETTE** magazines. Send large SSAE to John Buursema, 188 Oakview Road, West St. Paul, MN 55118. (612) 455-2197.

RAIL FAN TOUR: JUMPS-KITTS RAIL TOURS presents a steam doubleheader, on the Sierra Railroad, Sat. Nov. 4, 1978. Consolidation #28 and Mikado #34 depart Jamestown, center of California's famed "Mother Lode" at 9:30 a.m., return about 5:00 p.m. At least 5 photo run-bys are scheduled at points not accessible by car. Includes delicious buffet served enroute. Beverage service available: Adults: \$19.50, Children 3 to 7: \$12.50, Under 3 free. Genuine 1920-era Sierra Railway tickets to advance ticket purchasers. Send for information and/or tickets to: Jumps-Kitts Rail Tours, P.O. Box 816, Glendale, CA 91209. Please include self-addressed, stamped envelope.

commercial

FOR SALE: Miscellaneous HO, HOn3, On3, and Sn3. Write for list. SSAE John McMillan, 4786 Pineaire Lane, West Palm Beach, Florida 33409.

FOR SALE: SSLTD, FSM, Timberline II, Campbell, Athearn, magazines, books, annual reports at great savings. LSSAE for list. Michael Peart, 1239 Beach St., Apt. 9, Montebello, CA 90640.

GAZETTE: Vol. 1, 2, 3 complete, mint \$45.00 postpaid. Sell as set only. Jonathan Black, 409 Dorothy Drive, King of Prussia, PA 19406.

commercial

FOR SALE: HOn3, 26 brass locos, 12 brass cars, 60 cars, car and structure kits, trucks, extra parts. Send SASE for list. John R. Corea, 1745 Klamath Drive, Salinas, CA 93906.

BRASS FOR SALE: PFM Sou. PS-4 \$140.00, GN F-8 Black \$195.00, GN S-1 mint \$210.00, GN Q-1 \$275.00, D&RGW K-37 early run, \$140.00, AHM Milw. F-6 ptd. \$165.00, Sunset GN 0-4 mint, \$145.00, GN 0-1 custom painted \$160.00, NWSL D&IR N-3 ptd. \$195.00. Don Mills, P.O. Box 325, Seekonk, MA 02771.

FOR SALE: Free lance combine, removable roof. Write C. Morse, Box 758. Wareham, MA 02571.

FOR SALE: WP&Y container flat, wood kit with metal and plastic castings including trucks. Introductory offer \$7.00. Regular \$8.95. Converted standard gauge tank, made from Athearn 40' tank, includes dry transfers, truck and Kadee couplers, \$8.95 (available late Sept.) Eastern Scale Models, 22 Flamingo Crescent, Bramalea, Ontario, Canada L6T 2G5.

commercial

FOR SALE: On3 custom built D&RGW T-12, Kemtron, Grandt, Backshop castings, Micro Motor, PFM sound, \$450.00. D&RGW Rotary OM, powered rotor, PFM sound with tender and water car \$185.00. RGS three truck Goose, micro motor, \$170.00. 2 D&RGW passenger cars & one baggage, interior details \$85.00 1 Rotamec #998110 motor \$15.00, 2 Rotamec #997110 motors \$25.00, 1 Sagami #22361 motor \$12.00. A.G. Buckley, The Back Shop, Box 1263, Secona, AZ 86336 (602) 282-3747.

BOOKS: Current publishers, discount prices, LSSAE for list. Railroad equipment — NG & SL. P.O. Box 213, Lindsay, CA 93247.
commercial

FOR SALE: New locomotives and equipment. Westside AT&SF E-9 A&B \$100.00, Westside SP PA-1 A&B (Daylight) \$100.00, Key AT&SF #1050 2-6-2 \$100.00, GH Products Super push button II box of 8 \$20.00, Hallmark F-7 A-B-A shell set \$35.00, Westside can motor \$7.00, Gem 3 dome tank car \$15.00, Tenshodo Crown painted SP 4-8-8-2 \$850.00, Key SP 2-6-0 M-21 \$100.00, NWSL SP #1 diesel upgraded \$75.00. Used equipment. Alco RS-3 unpainted \$90.00, NWSL Shay HOn3 \$190.00, WSM SP 460 #8 \$110, WSM SP 460 #18 ptd. \$110, Balboa 20 T Shay HOn3 \$190, Tenshodo GP-35 ptd SP \$75.00, Hallmark GP-35 unpainted w/can motor \$30.00. Jess Adama, 1699 W. School, Porterville, CA 93257.

TRADE: Fine scale kit #115 or #135 for #110 or will purchase outright. All letters answered. Lyle Litzer, 714 Town Line Rd., Wausau, WI 54401.

WANT: at reasonable prices 1930 era or earlier On3 engines, cars, structures, etc. Suitable for logging and mining layout. Kits preferred unless RTR as manufactured. Will consider unfinished kits. All mail answered. Lyle Litzer, 714 Town Line Rd., Wausau, WI 54401.

FOR SALE: Over 30 books, mostly about narrow gauge, 25% to 50% off regular prices, back issues of the Gazette, WW&F bonds, etc. On3 available includes some Grandt Line parts, Kemtron C-16 2-8-0 modern kit, etc. Some HOn3 also available. Send 28¢ LSSAE for lists. George E. Pitchard, P.O. Box 576, new Milford, Conn. 06776.

FOR SALE: HO and HOn3 models, FSM, SSLTD, kits, back issues of RMC, MR & NGG. Write for list. Paul W. Ellis, 3904 Stanton, Colorado Springs, CO 80907.

WANTED: Leetown On3 disconnect log car kits, unbuilt. Steve Barmash, 12546 Woodstock Dr., Upper Marlboro, MD 20870.

FOR SALE OR TRADE: HO standard going into On3; NWSL Kosmos #11, Rayonier #70, PFM Ma & Pa #26, Westwood Ma & Pa kits, Central Valley archbar & four wheel passenger trucks, plus more. Steve Barmash, 12546 Woodstock Dr., Upper Marlboro, MD 20870.

FOR SALE: On3 D&RGW T-12 \$450.00. Extra details, painted. Bob-1-303-7956309, Littleton, CO.
commercial

FOR SALE: On3 Spartan American or Mogul, removable cab. Test run-guaranteed first class, \$155.00. Following units HO/ HOn3 new or TRO unless stated. NWSL 41' log car with trucks, box of three \$16.00. Hallmark CM tender \$23.00, Key D&RGW 583 \$113.00, Westside modern slide valve K-27 \$140.00, Westside C-16 #268 \$129.00, Lambert C&S 2-6-0 \$145.00, NJ C-21 \$123.00, NKP CB&Q 4-6-0 #113, also, unused with nice CB&Q custom paint \$125.00, United used RGS #20 \$130.00, United used Heisler with nice weathered paint \$142.00. Satisfaction guaranteed, all, US shipping paid. List for SSAE new and used brass-supplied at discount. Custom building in brass. Great Southern Works Rt. 1 Box 753 AA-105 E. 17th St. Reserve, LA 70084 (504) 536-4373.
commercial

WANTED: Fine Scale Miniature kits. Will pick up in the southern California area. Top dollar paid. Mike Hartley, 25532 Chrisanta Dr., Mission Viejo, CA 92691. (714) 837-1929.

FOR SALE: On3 Sunset C-16 painted \$350.00, Custom Brass cabooses \$80.00 each. Custom Brass GRAMPS tank \$85.00, USH 13T Shay TRO \$250.00. M.W. Long 307 N. Francis, Cedar Falls, IA 50613 (319) 277-3039.

FOR SALE: Pacific Northwest Region has maps, plans, trackwork and profiles of RGS, D&RGW, and D&RG. They date back to 1890 and range from one to thirteen square feet. These are now available for copying. For more information send a SSAE to N. Smith, PNR Librarian, 8115 20 S.W. Seattle, WA 98106.

FOR SALE: O scale USH NYC Niagara PTD TRO, USH NYC streamline Hudson & cars, An F3 A&B, sound, NYC, Alco S-1 TRO new, Lobaugh C&O 4-8-4 kit, car kits. SAE to Ron Morse, 1334 Uinca Way, San Diego, CA 92114. (714) 264-2465.
commercial

WANTED: GAZETTE Jan. 1977 issue. Please quote price. Carl Ignatz, 1617 Maple, Granite City, ILL 62040. (618) 876-4287.

NEXT ISSUE PREVIEW

Gordon North on Sound Part 3

All you will ever need to know about installing
sound in your locomotives.

**CAMS ★ PICKUPS
SPEAKERS ★ LOCO MODULES**

CHANGE OF ADDRESS

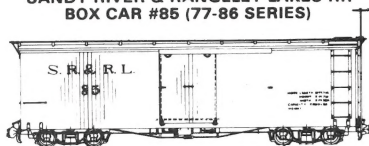
Please send us your **GAZETTE** mailing label and your new address. If you don't have your mailing label send your name and both your old and your new addresses. When you move let us know as soon as possible. Our mailing list goes to our data service for processing about the 20th of the month preceding issue. Address changes received after that date cannot be processed until the next issue.

On2 Model Railroad Car Kits

**SANDY
RIVER
CAR
SHOPS**

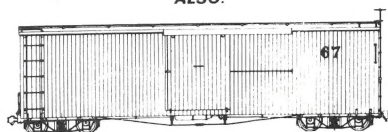


**NOW
SANDY RIVER & RANGELEY LAKES RR
BOX CAR #85 (77-86 SERIES)**



In HO n2 1/2 \$7.50
In On2 \$16.95

ALSO:



**WISCASSET, WATERVILLE &
FARMINGTON RAILWAY
BOX CAR #67 (TYPICAL 30' BOX CAR)
(Can be constructed as
Franklin Construction Co. Box Car.)
\$16.95**

On2 Ties (200 to a package) \$1.75

*Send a stamped self addressed
envelope for details.*

Mass. Res. please add 5% Sales Tax

SANDY RIVER CAR SHOPS
567 Rockdale Avenue
New Bedford, Mass. 02740

S/Sn3 is growing!

Here is where it's at in model railroading today. S scale is the ideal size, not too big, not too small. It's like blending the size of O gauge cars and locomotives with the operational possibilities of HO gauge.

Get into S/Sn3 — a little, or a lot. Read the S Gaugian magazine for ideas and product news. \$8.00 per year; \$9.25 outside USA. Single issue price: \$1.50.

We offer these other books for you:

Your Introduction to S Gauge — 76-page guide is filled with articles, photos, products available. \$3.50 plus \$.60 postage/handling.

S Gauge Building & Repair Manual — 76-page manual offers construction and repair articles. \$3.00 plus \$.60 postage/handling.

Cars & Locomotives — 44-page book details building cars and engines in S gauge. \$3.50 plus \$.50 postage/handling.

D&RGW Narrow Gauge Plan Book — 56-page (11x17-inch) book of official drawings of steam locos, freight, passenger and work equip. \$6.95 plus \$.50 postage/handling.

s gaugian Box 666
Tolono, Illinois 61880

IT'S MAILROADING TIME! THE NOW WAY TO MAILORDER MODEL RR EQUIPMENT

SERVICE and DEPENDABILITY have been the hallmark of our operation since the doors first opened in 1954. Our reputation as an honest, reliable dealer deserves your trust. Send us an order... You won't get damaged, used, or mishandled merchandise.

FAST SERVICE, NEWEST RELEASES, HARD TO FIND ITEMS, these are the benefits you get from Terminal Hobby Shop! We also include a **HOTBOX NEWSLETTER** with each order (keeps you in touch with what's new & exciting)

RAILROAD PLEDGE

- 1 To give you the **BEST SERVICE** we can today, and to try to do **BETTER** tomorrow.
- 2 To **CAREFULLY CHECK** your order to prevent errors.
- 3 And (because we are human), to **CORRECT ERRORS FAST**, with exchange, replacement, credit or refund as requested.

AMERICAN EXPRESS - MASTER CHARGE - VISA
(Minimum \$20 order for charge)

\$1.00 Handling (We pay postage on U.S. orders)

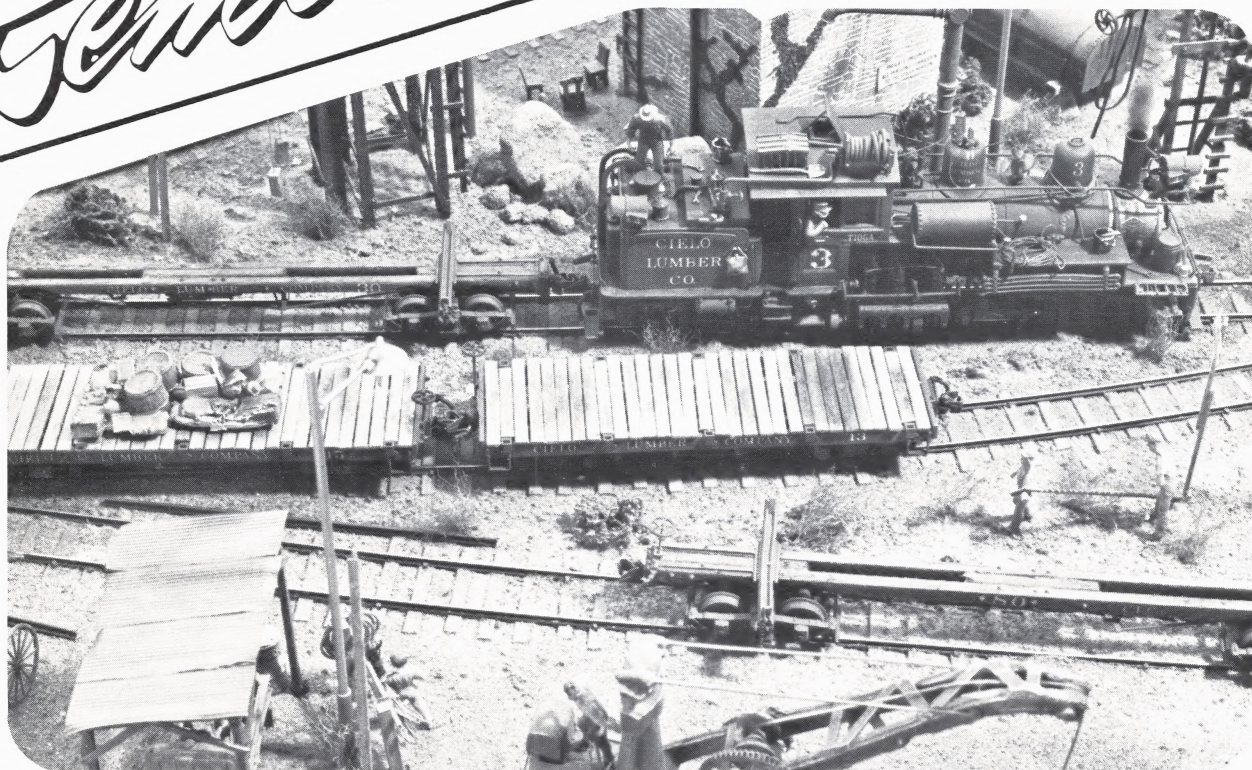
24 Hr Phone (414) 461-1050 (After-hour calls recorded)

TERMINAL HOBBY SHOP
5619 W FLORIST AVE MILWAUKEE, WI 53218

Alpine Publishing Ltd.	A.	68
The Back Shop	B.	71
Caboose Hobbies	C.	10
Caboose Stop Hobbies		19
Camino Scale Models		73
Chooch Enterprises		13
C.H.B. Models		8
Al Chione		74
Clover House		13
Colorado RR Museum		73
J. Conde		66
Coronado Scale Models		66
Darwin Publications	D.	15
Deer Creek Locomotive Works		19
Detail Associates		8
Douglas Models		7
Downtown Hobbies		68
E & B Valley	E.	12
Empire Midland Model		9
E.S.M.		61
Evergreen Hill Designs		24, 71
Evergreen Scale Models		13
Flag Stop	F.	14
Gemini Scale Models	G.	9
Gilpin Tram Line		61
Gold Spike Trains		19
Grandt Line		73
Green Lantern		61
Hetch Hetchy Models	H.	74
Iron Horse Hobby Supply Center	I.	74
Iron Horse Models		67
J.L.E.	J.	73
John's Train Shop		8
Kemtron Corp.	K.	79
Key Imports		45
Keystone Locomotive Works		9
M.F. Kotowski		73
The Little Depot	L.	13
Little Town Hobbies		19
Magnuson Models Inc.	M.	73
Model Masterpieces		13
Motive Power Equipment & Supply		9
Narrow Gauge & Shortline Gazette	N.	68
National Motors		67
N.J. International		2
Northwest Shortline		14, 72
Oliver Steam Rail & Navigation	O.	25
Original Whistle Stop		9
Peninsula Hobbies	P.	15
Peterbuilt		25
P.F.M.		70
Power Systems Inc.		71
Precision Scale		75
Rail Craft Products	R.	35
Reed's Hobby Shop		53
Right-O-Way		71
Rio Grande Models Ltd.		72
Rittech		6
Rogers Locomotive Works		8
Ron's Art Graphics		61
Ron's Car Shop		71
Roy's Train World		72
Sandy River Car Shops	S.	78
Sango		6
Irv Schulz "125 Line"		25
S Gaugian		78
Silver San Juan		13
Robert Sloan		71
Structure Co.		61
Sundance Publications		10
Sunset Models		65
Terminal Hobby Shop	T.	78
Tomalco		6
Toy & Hobby House		35
The Train Exchange		35
Trains & Track		8
The Train Works		68
Troller Corporation		4
V & T Shops	V.	68
Vintage Reproductions		61
Western Guideways	W.	74
Westside Models		80
Thomas A. Yorke	Y.	11, 61

Kemtron

JUST FOR YOU



JOHN OLSON PHOTOGRAPHY

KEMTRON'S LATEST RELEASES IN HO_N3 ARE HERE

Superbly detailed brass and styrene Log Cars and Flat Cars, patterned after the popular prototypes of Swayne Lumber Company and Westside Lumber Company in California's Sierra Nevada. Either of these car kits will go the distance in filling out your equipment roster. Better yet purchase three at a time and add them by the string.

HO_N3 Skeleton Log Car Kit4210
Box of 34310

Easy assembly with ACC cement ... best logging trucks available ... cast brass bolsters and side frames provide weight near the rails ... Delrin[®] brake beams and shoes for faultless operation ... detailed cast brass log bunks, brake wheel and air hoses ... injection-molded styrene main frame with crisp nut-bolt-washer detail ... less couplers.

HO_N3 24' Flat Car Kit . 4211
Box of 3 4311

Injection molded styrene car frame comes complete with stake pockets cast in place ... holes drilled for queen posts ... coupler pockets for your favorite couplers ... lost wax brass end beams, queen posts and brake wheel ... highly detailed brass trucks (with Delrin brake rigging) keep this little brute firmly on the rails even without a load.

Our expanding HO and O scale Illustrated Parts Catalogues are always available. Only \$2.00 each from your local dealer or direct. California residents add 6% sales tax.

KEMTRON CORPORATION

P.O. Box 360

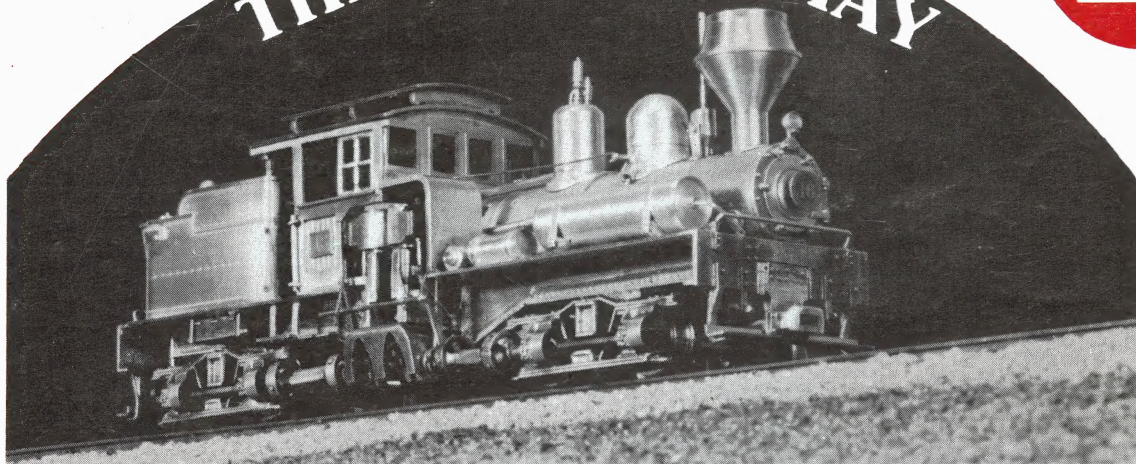
Walnut, California 91789

WESTSIDE MODEL COMPANY

3085 Carriker Ln. Soquel, CA 95073



THE ALI-SAN SHAY



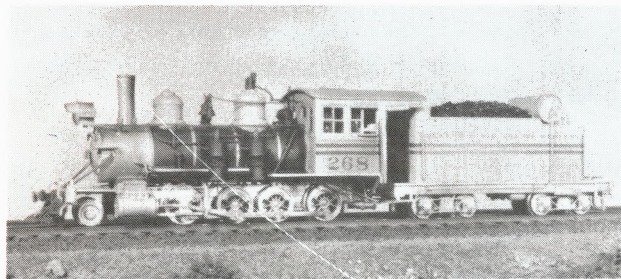
Available soon in HO standard gauge. Coming soon to your local dealer.

No. 268

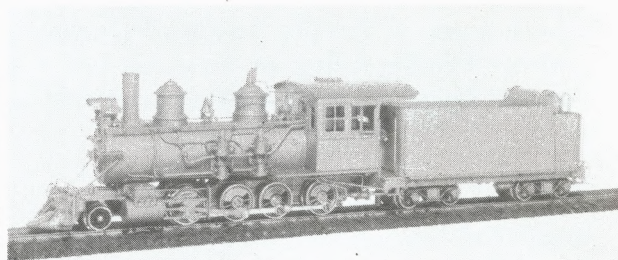


No. 278

THE BEST C-16 IN HO_{n3}?

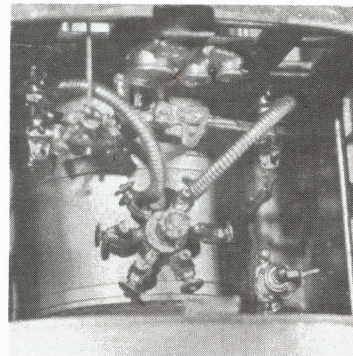
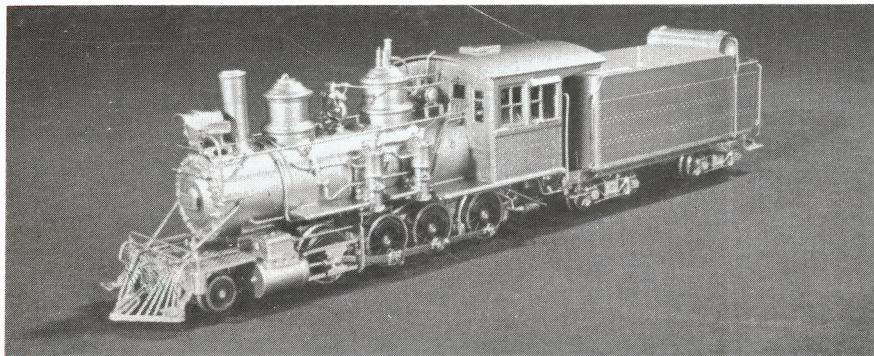


Painted by Mike Davis



Model Photography by Paul Jansen

No.'s 268 and 278 with standard motor are priced competitively with available kit versions.



WESTSIDE's "**New 278**" is equipped with coreless motor and full cab detail by PSC. This is the best C-16 built so far in looks, assembly and performance.

A lot of C-16's have been on the market in On₃, Sn₃, and HO_{n3}, but WESTSIDE's C-16 has led the way with over 3,000 in service on model railroads.